

# ***Callander Downtown Waterfront Revitalization Master Design Strategy***

*Final Report  
February 2012*



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***Final Report  
February 2012***

Prepared For:  **Callander**  
ONTARIO  
*Four Seasons of Reasons*

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*in association with*

  
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advice • strategy • implementation

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# executive summary

The Municipality of Callander is pursuing revitalization of the Downtown Waterfront Area through the preparation of a Downtown Waterfront Revitalization Strategy and Community Improvement Plan. This Strategy was initialized for the purpose of revitalizing the Downtown Waterfront Area to better service a growing population within the Municipality. It will enable the Municipality to revitalize and enhance the community as well as promote investment opportunities in the community's commercial / industrial and downtown waterfront core in order that new economic growth and job creation can take place. This report provides the Final Revitalization Master Design Strategy for the Downtown Waterfront Area. The Community Improvement Plan, contained in a separate document, accompanies this Strategy.

Phase 1 of the Callander Downtown Waterfront Revitalization Strategy included a review of the existing planning framework, completion of various analyses including site, engineering and building assessments, cultural and natural heritage context evaluation, market analysis, and other supportive technical analyses. Phase 1 also consisted of an extensive community engagement and consultation process. This included an initial community listening session with over 30 people in attendance, stakeholder focus group sessions, meetings with Municipal Council, Committees and Staff, as well as a questionnaire that was mailed to the residents of the Municipality and garnered 40 responses.

The completion of Phase 1 work paved the way for Phases 2 and 3 of the Strategy which include design guidelines, the final Revitalization Strategy, the Community Improvement Plan (CIP), funding and finance strategies and other implementation-oriented action steps. Taken together, Phases 1, 2 and 3 of the Downtown Waterfront Revitalization Strategy will recommend, and with approval of Council, result in tangible improvements in the urban area of the Municipality.

Key findings from Phase 1 include the following summary items:

- The Municipality has a number of properties in its ownership within the study area, providing the potential for the development of public spaces. There are also a number of privately-owned properties that have been identified within this report as potential redevelopment sites.
- There is a need for affordable housing options within the Downtown Waterfront Area. The Affordable Housing Needs Assessment Study (2009) recommends that 25% of future housing should be row or townhouse development and 15% should be apartment units.
- At present, there is little remaining capacity in the sewage lagoon system in Callander.

- The north-south leg of Callander Wharf is severely damaged, unusable, and beyond economical repair in its present condition. The east-west pier is determined to be in good condition; while it is recommended that the wood decking is to be replaced between the cribs.
- There are a number of sensitive natural features in the area surrounding the Downtown Waterfront Area, including Provincially and Locally Significant Wetlands and Sensitive Fish Habitats. These shall be protected from incompatible land uses and activities.

Based on these analyses, the study identified opportunities for the enhancement of the downtown and waterfront areas. These opportunities have been addressed in the work carried out in the Final Downtown Waterfront Revitalization Strategy.

The Final Revitalization Strategy Report includes an overview of the study objectives, context, opportunities and constraints identified through the background study conducted in Phase 1. It outlines the vision and design principles that are the key guiding elements of the Master Design Strategy. The Strategy contains overall revitalization concepts related to streetscape improvements, main intersection enhancements, waterfront development, park improvements, defined signage, development / redevelopment sites, potential parking areas and preferred truck route.

Design guidelines are identified, based on design themes deeply rooted in Callander's natural and cultural heritage - specifically the nautical and logging heritage. Design concepts are provided for Lansdowne Street, Main Street, Centennial Park, and Waterfront Place.

A comprehensive implementation strategy is also included in this report, outlining strategic projects and prioritization, potential economic benefits, capital cost estimates, funding sources, issues and impacts, and management requirements. This report also provides sustainability considerations related to site development to ensure that Callander continues to be a "green" community.



# 1.0 introduction





## 1.1 Background and Study Purpose

Callander began as a logging and sawmill settlement, however the community's character was greatly altered when it became the centre of international attention and a major tourist attraction after the birth of the Dionne quintuplets in the nearby village of Corbeil in 1934. With several tourist lodges and camps on Callander Bay, the community has since become the jumping-off point for both summer and winter water-based recreational activities for residents and visitors alike.

The Municipality of Callander retained the Consulting Team of EDA Collaborative Inc, Sierra Planning and Management, and W.F. Baird & Associates Coastal Engineers Ltd. to prepare a Downtown Waterfront Revitalization Master Design Strategy and Community Improvement Plan.

The purpose of this study is to enable the Municipality of Callander to revitalize and promote investment opportunities in the commercial/industrial and downtown/waterfront core for new economic growth and job creation. Ensuring that the Municipality is able meet future demands for public, social, health, housing and recreational services in a sustainable manner will further enhance the quality of life for residents, business owners and visitors.

This strategy is also intended to serve as a guide for Council and Municipal Administration for the prioritization and budgeting of future staged capital improvements within the Downtown Waterfront core area.

## 1.2 Study Objectives

The overall objective of the Downtown Waterfront Revitalization Master Design Strategy is to evaluate the feasibility of incorporating a number of components and considerations which will effectively integrate the remarkable waterfront assets with the Downtown Area to enhance Callander aesthetically, environmentally and commercially. Specific study objectives include:

- Develop a themed streetscape concept plan for Main and Lansdowne Streets;
- Improve and maximize use of waterfront assets;
- Investigate the potential for marina services expansion;
- Strengthen the local economy through diversifying the business mix and facilitate the creation of additional employment opportunities;
- Explore and identify opportunities for affordable housing development;
- Identify additional parking efficiencies;
- Identify specific streets to serve as preferred service routes through core area;
- Provide capital funding estimates and funding source suggestions;
- Develop concept plans for all elements proposed in Master Plan; and,
- Provide realistic projections for increased construction and permanent employment opportunities.

*“The purpose of this study is to enable the Municipality of Callander to revitalize and promote investment opportunities in the commercial / industrial and downtown / waterfront core for new economic growth and job creation.”*



# introduction

## 1.3 Study Area

The Downtown Waterfront Revitalization Master Design Strategy study area is defined in the Official Plan as the Downtown Waterfront Area. This area is bound by Burritt Street and lots along Lansdowne Street to the north, View and Bay Streets to the south, Highway 94 to the east, and Callander Bay to the west (see Figure 1). The study area also includes the two public open space areas located between Callander Bay and Main Street at the northern and southern edges of the Downtown Waterfront Area.

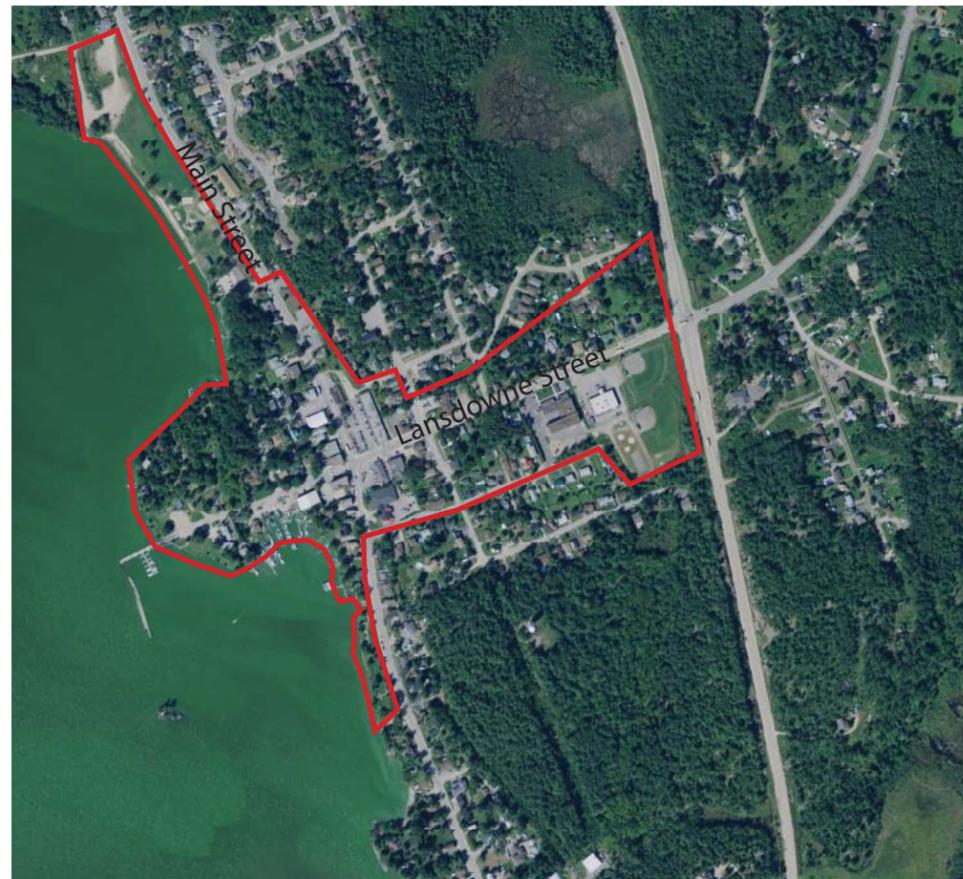


Figure 1: Callander Downtown Waterfront Revitalization Study Area

## 1.4 Study Process

The Downtown Waterfront Revitalization Master Design Strategy was conducted over three phases.

Phase One – Background, Analysis and Vision was launched in November 2010. A site reconnaissance and start-up meeting was held on November 27th, 2010 with the Client Team and the Consulting Team. Background reports, mapping and related research were reviewed and synthesized together with a detailed site investigation, inventory and analysis. Key opportunities, constraints and issues were identified for discussion and to serve as a foundation for preparation of the Master Design Strategy.

Phase Two – Preliminary Strategy and Implementation Options focused on the preparation of detailed alternative development scenarios for the overall Downtown Revitalization Strategy for review and evaluation. A preferred concept was prepared accompanied by a series of design guidelines to articulate key directions and objectives for the plan. These included guidelines related to streetscapes, parks, open space and trails, built form and facades, and signage. Capital development costs of the recommended public realm features and elements were also prepared.

Phase Three – Final Revitalization Strategy focused on the preparation of the final Downtown Waterfront Revitalization Master Design Strategy and Implementation Strategy. Once the Strategy was finalized, and based on feedback from the Client Team, the Community Improvement Plan (CIP) was prepared.

*The Downtown Waterfront Revitalization Master Design Strategy was conducted over three phases:*

*Phase 1 - Background, Analysis + Vision*

*Phase 2 - Preliminary Strategy + Implementation Options*

*Phase 3 - Final Revitalization Strategy*

2.0

context, opportunities + constraints





## 2.1 Location

The Municipality of Callander is strategically located in northeast Ontario at the southeast corner of Lake Nipissing on Callander Bay (refer to Figure 2). The Municipality is approximately 332 kilometres north of Toronto and borders the City of North Bay.

## 2.2 Community Profile

The Municipality of Callander is predominantly rural, with the Callander Settlement Area being the only urban area within the jurisdiction. The Callander Settlement Area acts as a service centre for the surrounding rural areas, particularly those areas located to the south.

The Municipality is home to 3,579 permanent residents with a projected increase to 4,579 by 2026, with an anticipated growth in residential housing of about 20 units per year. A majority of this growth is expected to locate in the Callander Settlement Area, which includes the Downtown Waterfront Area.

The current demographic profile of the Municipality of Callander is that of permanent residents, seasonal mainland and island cottagers, as well as a recent influx of newly-arrived young couples and families looking for the peacefulness and tranquility that Callander offers.

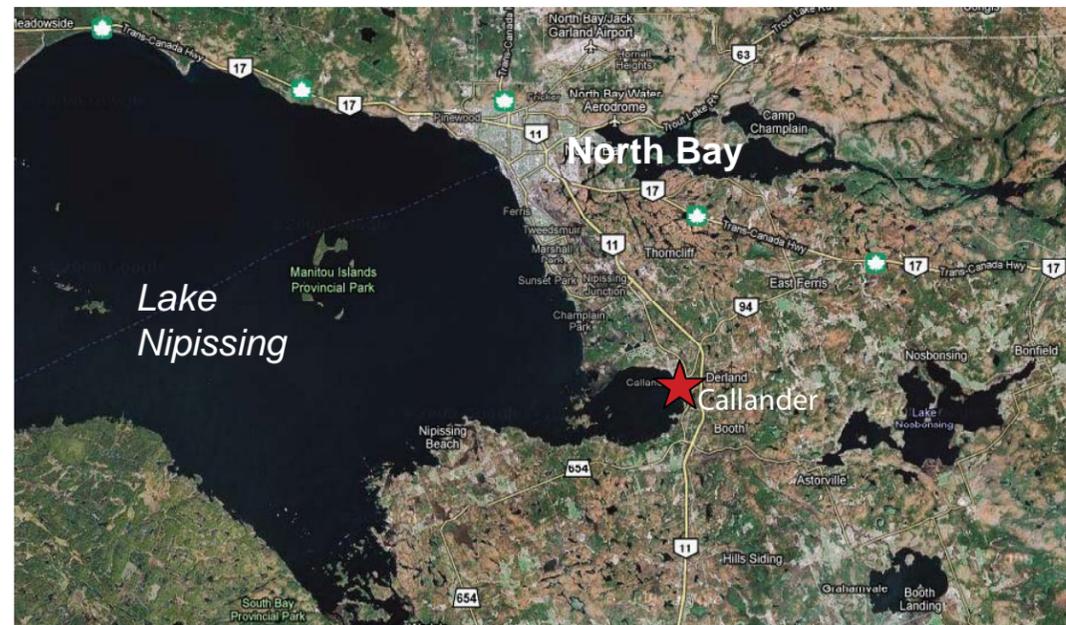
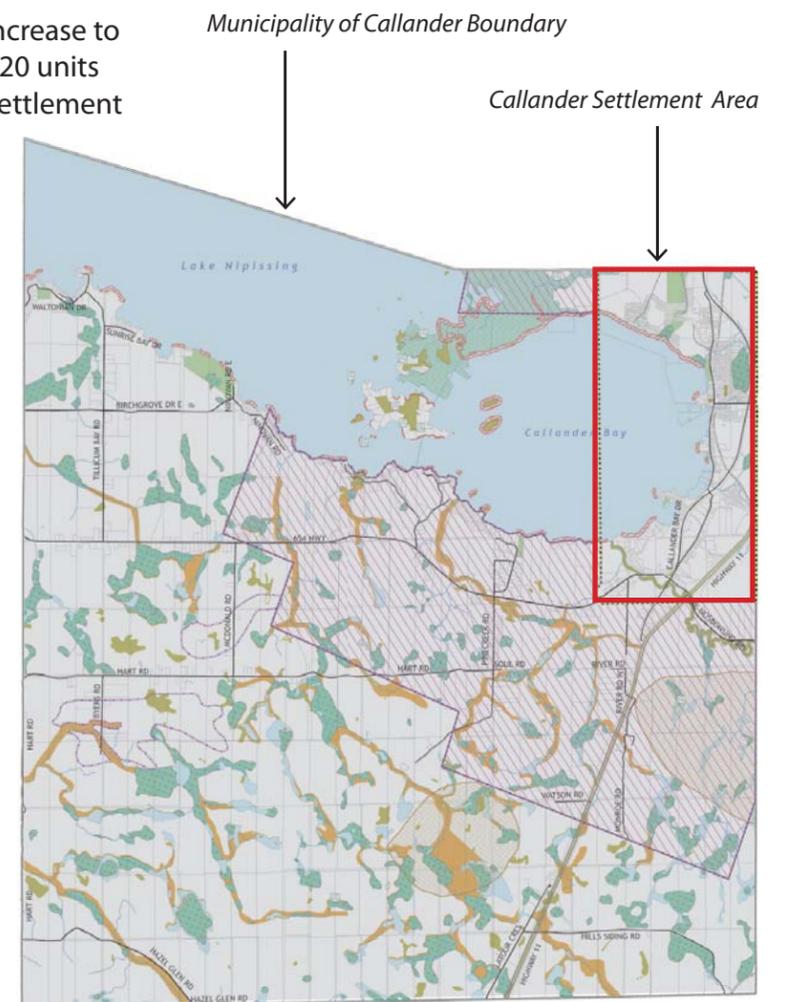


Figure 2: Location Map of Callander



Main Street Pedestrian Realm



Residential Street

# context, opportunities + constraints

## 2.3 Urban / Rural Connections

The Municipality of Callander contains a large amount of rural land outside of the urban area, defined as the Callander Settlement Area in the Official Plan. As identified in Section 1.0, the study area for this strategy is the Downtown Waterfront Area, located within the Settlement Area boundaries; however, a number of Municipal assets are located outside of the study area that are important to the development of the overall strategy. Ensuring that these assets are connected or linked in some way to the Downtown Waterfront Area is an important consideration. Figure 3 illustrates the urban / rural connections.

These include the following:

- |  |                           |
|--|---------------------------|
| ① Downtown Waterfront Area                               | ← 6 To Trans Canada Trail |
| ② Yarlasky Park / Community Centre / Bill Barber Complex | ⋯ 7 Powassan Circle Route |
| ③ South Shore Community Centre                           | ⋯ 8 Cranberry Trail       |
| ④ Callander Bay Wetlands Conservation Reserve            | ⋯ 9 Kate Pace Way         |
| ⑤ Nipissing Beach  | ⋯ 10 Corbeil Circle Route |
|  | ⋯ 11 Shannondale Trail    |

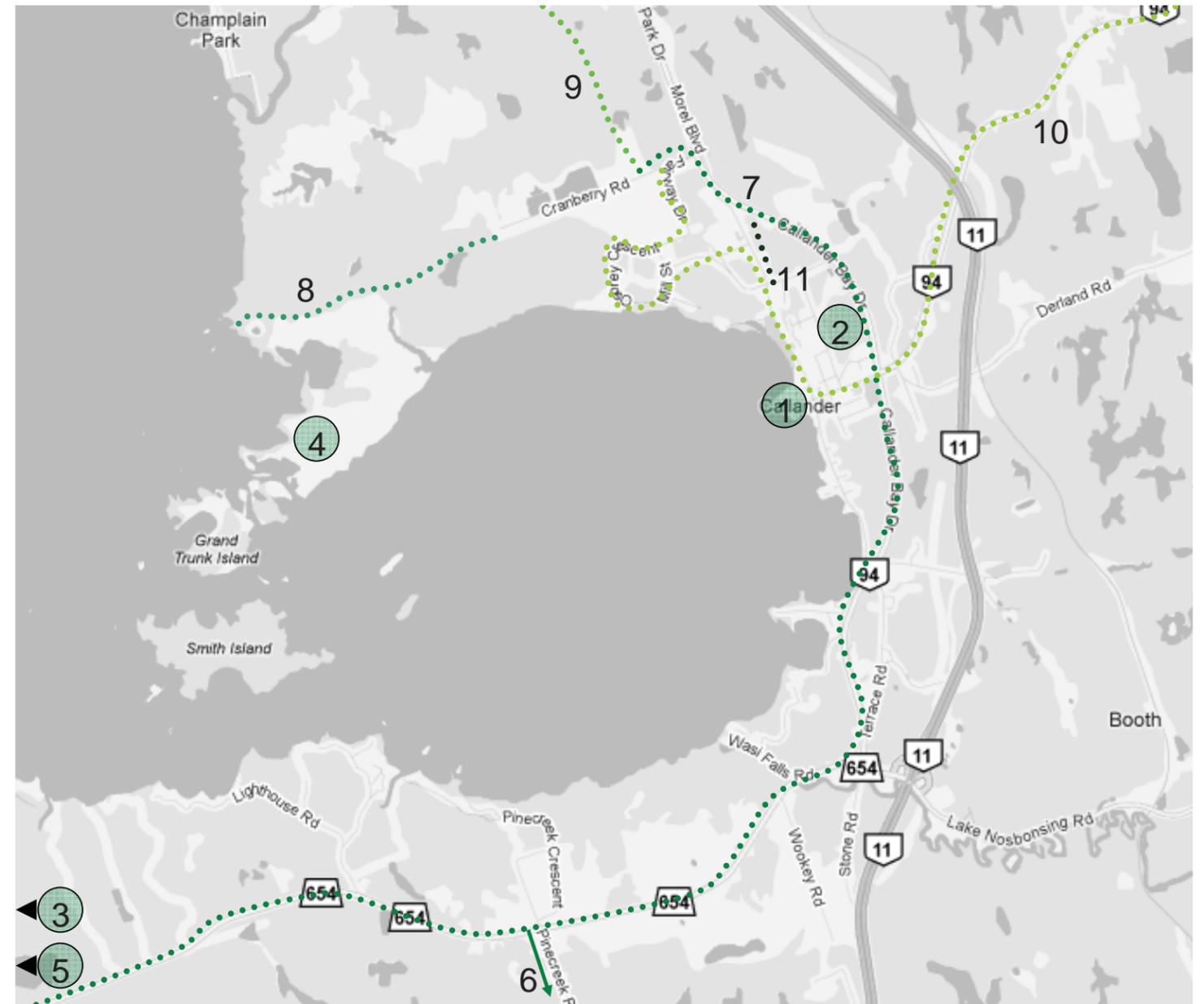


Figure 3: Urban / Rural Connections

## 2.4 Key Features of Downtown Waterfront Area

Within the Downtown Waterfront Area there are a number of facilities, amenities and services which can contribute to the overall vibrancy of the community. The main intersection (Main and Lansdowne Streets) within the study area is the commercial core of Callander, with a number of retail and service businesses located here. On-street parking is permitted on the west side of Main Street from Lansdowne Street to Centennial Park and on the east side in front of the commercial plaza just north of Lansdowne Street.

Along the waterfront there are two parks, Centennial Park and Memory Tree Park, and the Callander wharf is centrally located at the west end of Lansdowne Street, however it is in a state of disrepair. There are a number of marinas just east of the wharf and a boat launch is located to the north of the wharf. Along a substantial portion of the waterfront, particularly in the area near the wharf and south of Centennial Park, there is a municipally-owned waterfront right-of-way that also connects with Lansdowne Street (see Figure 4). Centennial Park currently has a playground, picnic shelter, break wall, parking area, boat launch and beach, and acts as the staging area for ice fishing during the winter months. These two waterfront parks provide excellent views to Callander Bay, while Centennial Park provides public access to the water.

The primary entrance into the Downtown Waterfront Area is from Highway 94 via Lansdowne Street. Other secondary entrances are along Main Street at the north and south limits of the Downtown Waterfront Area. Figure 4 illustrates the key features of the Downtown Waterfront Area.

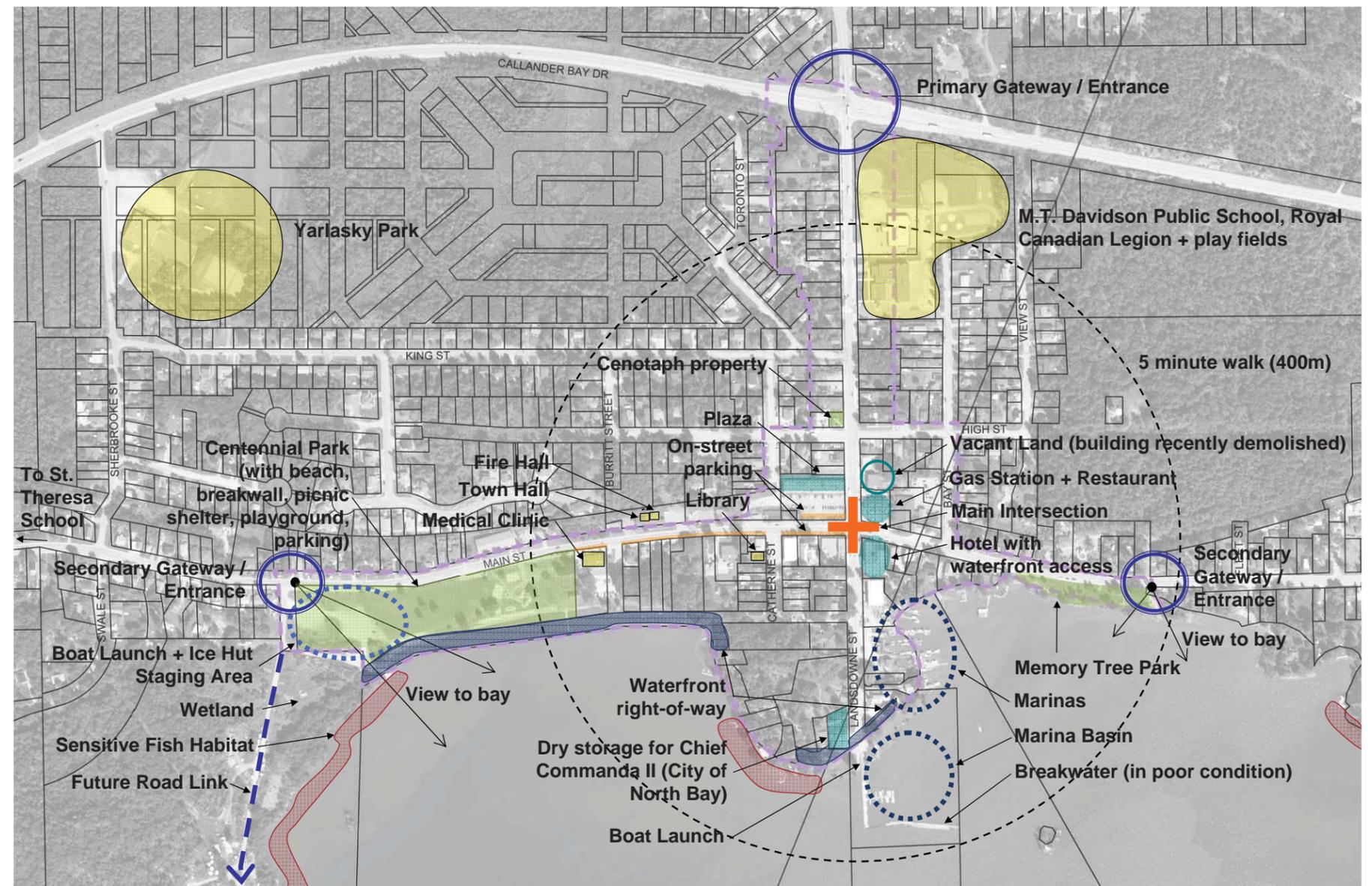


Figure 4: Site Analysis Diagram

# context, opportunities + constraints

## 2.5 Planning Framework



View of Lake Nipissing from Callander Wetlands Conservation Reserve

A solid foundation for developing a Master Design Strategy for the revitalization of Callander's Downtown Waterfront Area involved a comprehensive understanding of existing planning policies and guidelines as well as an understanding of the community itself from an economic, demographic, sociocultural and environmental perspective. A number of reference materials and documents were researched and analyzed including:

### **Policy Framework**

- Places to Grow - Growth Plan for Northern Ontario (2011)
- Municipality of Callander Official Plan Update (2010)
- Municipality of Callander Zoning By-Law 2003-857 (2003)
- Municipality of Callander Strategic Plan (Revised 2008)
- Community Improvement Plan for Central Callander (1991)

### **Economic Framework**

- Proposed Economic Development Strategy (Draft) (2010)
- Development Charges Background Study (2009)

### **Servicing and Infrastructure Framework**

- Wharf Structural Assessment and Costing (2009)
- Road Needs Study (2010)

### **Special Studies**

- Affordable Housing Needs Assessment Study (2009)
- Growth Management Discussion Paper (2008)
- Corporate Brand Identity Signage Program (2005)

A summary of these documents can be found in Appendix 1.



Figure 5: Callander Wharf Location Plan

## 2.6 Natural Features

The Municipality of Callander contains a number of natural heritage features, some of which are located within the Downtown Waterfront Area. There are also a number of sensitive natural features in the area surrounding the Downtown Waterfront Area, including Provincially and Locally Significant Wetlands, Sensitive Fish Habitats as well as a number of watercourses. These features are to be protected from incompatible land uses and activities to ensure that no negative impacts on the natural features or their ecological functions occur.

## 2.7 Coastal and Shoreline Features

The key central feature of Callander's waterfront is the L-shaped wharf, as illustrated in Figure 5. The wharf is over a century old and over the past several years, the condition of the wharf has deteriorated such that the north-south leg of the wharf is severely damaged, unusable and beyond repair in its present condition. The east-west pier is determined to be in good condition; while the wood decking is recommended to be replaced between the cribs. It was also determined that the boat ramp immediately north of the wharf is in a state of disrepair and should be demolished and replaced. Significant reinvestment is required to rehabilitate this important waterfront feature.

Additional waterfront issues have also been identified, including shoreline protection, stormwater and outfalls, private riparian shorelines, and intake protection zones; these are addressed in Appendix 2.

Assessment of the coastal and shoreline conditions in Callander was undertaken in Phase 1; the entire Coastal Engineering Assessment can be found in Appendix 2.

## 2.8 Demographics

Callander is a desirable residential area and has experienced a steady increase in its population since 1996, with significant growth between 2001 and 2006 when the population growth rate was 12.7% - almost twice as high as the provincial rate and significantly higher than that of Parry Sound District (3.2%) and North Bay CA (1.8%). Callander's population is projected to continue growing by 50 persons annually from 2006 to 2026, provided that improvements are made to the sewage treatment facility according to the 2008 Growth Management Discussion Paper prepared for Callander.

The strong residential growth in Callander can be attributed to some key elements:

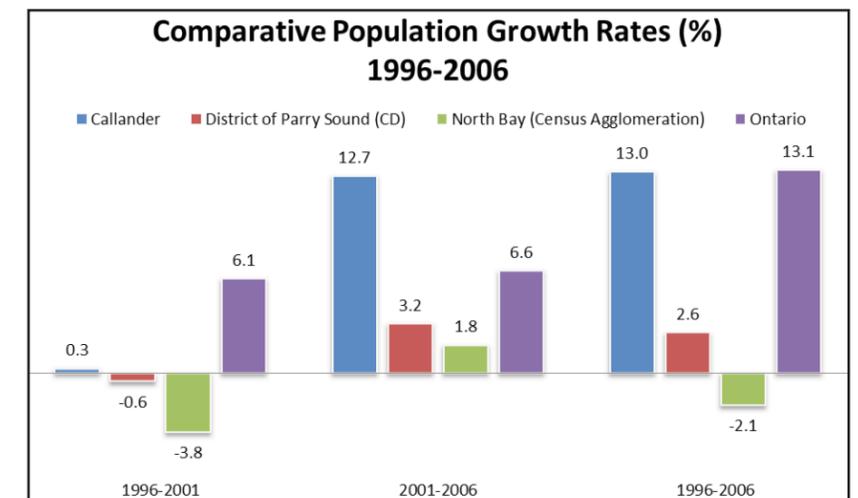
- Strategic location in Ontario's near north – next to a major regional service centre with all the advantages of being close to a large urban centre but with easy access to a rural setting;
- Naturally attractive setting with appealing views;
- Offers a range of benefits (health care services and regional hospital nearby or in the community) to those with family relations in southern and northern Ontario;
- Property tax advantage over nearby North Bay;
- Callander is essentially a midpoint between northern and southern Ontario, especially in the near future when the new highway is completed and the journey to Toronto is even more accessible.

## 2.9 Housing Market

The current housing landscape in Callander is dominated by low density, single-family, single-detached housing. Not surprisingly, housing tenure is also skewed towards home ownership with just 13.5% of households renting, and, according to the 2009 Affordable Housing Study, very little rental supply exists in the Municipality.

Building permit data shows a very active residential development sector. From 2005 to 2010, there were 169 residential building permits with a total value of over \$36 million. According to the Municipality of Callander, new development in the last five years has been entirely single-family single-detached dwellings. This situation, combined with the lack of rental and social housing supply in Callander, suggests that the housing market is skewed to the ownership market and newly built housing is geared towards more affluent buyers. As a result, very little options exist for those unable to purchase homes.

The steady increase in new developments has also placed a strain on the sewage treatment infrastructure in Callander, and a lack of sewage capacity is a barrier to further development.



Source: Statistics Canada 2006 Community Profiles



# context, opportunities + constraints

## 2.10 Key Findings from Community Engagement Process

Recognizing the strengths and identifying the opportunities that the challenges offer, a number of specific suggestions and key findings emerged through the community engagement process:

*Infrastructure development* *Gathering and viewing locations for events / festivals*  
*Community safety* *Aesthetic streetscape improvements along Main Street* *Access to public dock* *Keep /expand existing marina - new floating dock* *Better promote Callander* *Encourage presence of Chief Commanda II* *Reinstate 'gateway' structure* *Highlight quality heritage buildings* *Promote geological history and logging history* *Generate business activity - restaurants, boutiques, cafes* *Summer theater / building* *Docking for resort customers* *Establish a truck route* *Signage and marketing* *Enhance facilities for passive + active recreational use* *Multi-use pedestrian / bicycle paths* *Park / lookout point by Callander Wharf* *Seasonal fun rentals* *Attract 'transient' boaters*

In addition, a number of specific suggestions regarding additional or new amenities for Centennial Park emerged, particularly through the information received from the students at M. T. Davidson Public School.

# 3.0

## vision and design principles





### 3.1 Vision

A vision statement was prepared for the Callander Downtown Waterfront Area based on the key directions and outcomes of the community consultation and analysis:

***“The Village of Callander Downtown Core Area and Waterfront is envisioned as the ‘Village on the Bay’ that will create a cohesive community identity on Callander Bay that strives to protect and enhance the rich natural environment and human heritage; provides a strong framework for social interaction, land and water-based recreation and tourism; and encourages mutually supportive investment in the public realm and private development.”***



*Sustainability: The Triple Bottom Line*

### 3.2 Guiding Principles

Many of the concepts that were articulated during the stakeholder interviews, focus groups and community sessions were further developed from a conceptual design perspective as a result of the community design charrette and initial community listening session.

Based on the two community sessions, held in March and May 2011, a number of design principles were formulated based on the aspirations and values of the community. These principles served as the foundation upon which the Master Design Strategy was developed for the Callander Downtown Waterfront Area. These include:

- Make Callander ‘A Destination of Choice’
- Promote cultural excellence
- Further develop Callander as a gateway / hub
- Encourage year round uses
- Promote Callander as a green and sustainable community
- Retain / enhance marina facilities and services
- Enhance recreational facilities
- Identify and promote economic development opportunities

A preliminary conceptual framework for the design strategy was developed based on these design guiding principles.

# vision and design principles



## *Make Callander a "Destination of Choice"*

Enhance the existing core area to create a vibrant, safe and picturesque community. Ensure that the core area is pedestrian-oriented and accessible to all to create a sense of belonging and provide areas for community gathering and social interaction. Expanding the range of uses within the Downtown Waterfront Area and improving the streetscape, including sidewalks, lighting, street furniture, etc., will attract visitors, boosting the local economy and enrich the livelihood of both permanent and seasonal residents.



## *Promote Cultural Excellence*

Encourage and promote cultural opportunities to locate or take place within the Callander Downtown Waterfront Area. Ensure that there is adequate indoor / outdoor gathering and events space for festivals, markets, theatre performances, concerts, etc. The preservation of key heritage elements within Callander is also important. Ensuring that these cultural and historic features are embraced and celebrated within Callander will facilitate civic pride of these unique features.



## *Further Develop Callander as a Gateway and Hub*

Develop Callander Downtown Waterfront Area as a gateway to / from Callander Bay and Lake Nipissing. Establishing a system of gateways and signage will enable visitors to become easily oriented and have a pronounced sense of place. Providing adequate community services, facilities and amenities to ensure that Callander serves as a hub, or service centre, for residents, cottagers, tourists and visitors of all types during all seasons is imperative to create a distinct community on Lake Nipissing.



## *Encourage Year Round Uses*

Actively promote and encourage year round recreational activities, leisure opportunities, and cultural and entertainment events within Callander to enhance the quality of life for residents, tourists and visitors alike. Winter tourism opportunities should be promoted in order to sustain the tourism industry in all seasons. Opportunities related to ice fishing, snowmobiling, cross country skiing, snowshoeing, ice skating, etc., should be investigated.



***Develop Callander as a Green and Sustainable Community***

Callander should be promoted as a sustainable community, environmentally, economically and socially. Protecting the surrounding natural environment, enhancing the pedestrian environment, encouraging the use of active modes of transportation, encouraging sustainable practices in all new development, and providing interpretive signage regarding sustainable elements of new development will increase “ecological literacy” within the community enabling Callander to pride itself on the unique environmental surroundings that are an important part of the community.



***Enhance Marina Facilities and Services***

Retain and enhance the marina facilities and services within the Callander Downtown Waterfront Area. Improvement / replacement of the Callander wharf should be undertaken to enable Callander to promote itself as a destination on Callander Bay and beyond to Lake Nipissing. Providing adequate facilities and services for boaters and cottagers is an important aspect of the overall economic development and sustainability of the community.



***Provide Multi-Use Community Recreational Facilities***

Further develop and enhance recreational facilities within the Callander Downtown Waterfront Area that will create the opportunity for active transportation and recreation. These facilities include a well-linked multi-use trail system within the community with connections to the Kate Pace Way and regional trail system, as well as surrounding communities; an improved Centennial Park with expanded facilities; and, an enhanced waterfront area at the foot of Lansdowne Street with improved facilities and public spaces.



***Identify and Promote Economic Development Opportunities***

Promote the development of economic opportunities and mixed-use development within the Downtown Waterfront Area. Promoting tourism development, such as accommodations, services and amenities, will provide employment and increase visitors to Callander. Professionals (i.e., Engineers, Architects, medical professionals, etc.) should be encouraged to locate offices within the Downtown Waterfront Area to further develop the economic base.

Develop a diversified housing strategy that addresses young families and seniors needs. This population will help to sustain businesses throughout the year. Providing support to appropriate private sector development is also important to advance the economy within Callander.



4.0

master design strategy + guidelines





### 4.1 Design Strategy Conceptual Framework

A preliminary conceptual framework diagram was established based on the vision and guiding principles developed for the Callander Downtown Waterfront Area through the consultation process and associated analysis. This framework is centred on the two major streets within Callander, Main Street and Lansdowne Street, and anchored by the parks / open spaces found at the north, south, east and west boundaries of the Downtown Waterfront Area. A central feature is the main intersection where these two prominent streets intersect, comprising the commercial core of the Callander Downtown Waterfront Area. Other features identified include town gateways, core area gateways, access to parks and open space, continuous pedestrian linkages along the waterfront (to the greatest extent possible), core enhancements along the two major streets, and linkages to regional trails and assets throughout the Downtown Waterfront Area. Figure 6 illustrates the conceptual framework of the design strategy.

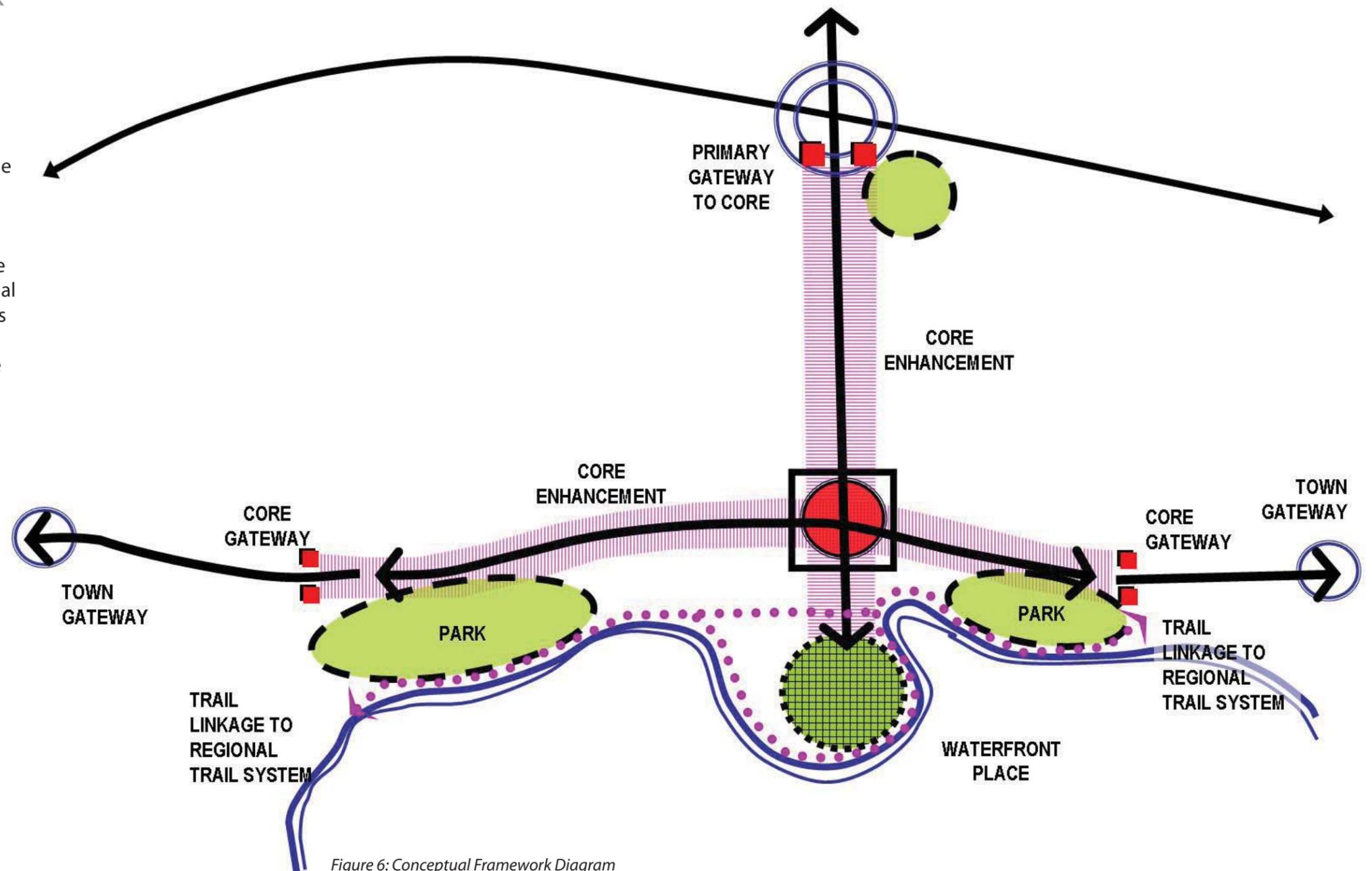


Figure 6: Conceptual Framework Diagram

# master design strategy + guidelines

## 4.2 Revitalization Concepts

The overall Revitalization Master Plan is illustrated in Figure 10 on page 29. A number of concepts for Callander Downtown Waterfront Area's revitalization were developed based on the outcomes of the community engagement process together with the analysis work completed by the Consulting Team. The Master Plan is described according to the following components:

### 4.2.1 Streetscape Improvements

Lansdowne and Main Streets, the two major streets within the Callander Settlement Area, have been identified as being in need of improvement to the right-of-ways to create safe and pedestrian-oriented streetscapes.

- Provide enhancements to the pedestrian realm along Lansdowne and Main Streets including street tree planting, street furniture, and on-street parking.
- Provide enhancements to the Cenotaph property at Lansdowne and High Street to create a passive recreational node and to ensure year-round use.



### 4.2.2 Main Intersection Enhancements

- Enhance the pedestrian crossings at the intersection of Lansdowne and Main Streets for increased pedestrian safety.
- Develop a small parkette / sitting area on the northeast corner of the intersection as a small social meeting place.
- Provide general enhancements to the streetscape with an overall focus on the enhanced intersection.
- Provide wayfinding and signage for orientation of visitors, tourists and residents.



**4.2.3 Waterfront Development**

- Improve wharf and marina for increased capacity and public use of the waterfront.
- Develop Waterfront ‘Place’ as a public facility with gathering spaces, boardwalk, improved boat launch (eventually relocated), picnic areas, etc.
- Provide a pedestrian friendly streetscape link to the downtown core via Lansdowne Street.
- Provide waterfront pedestrian links continually along the waterfront. It is noted that the location of private lands along the water’s edge prevents this continuous link from being fully realized. In areas where private land is on the waterfront, public right-of-ways and sidewalks shall be utilized where pedestrian connections are needed.



**4.2.4 Centennial Park Improvements**

- Enhance the beach area including clean-up and regular maintenance.
- Provide improvements to the recreation facilities currently located in Centennial Park to ensure a wide variety of passive and active recreational uses.
- Provide organized event parking / ice hut staging area.
- Improve the boat launch to accommodate non-motorized uses.
- Re-establish naturalization of the stream (currently running through parking lot) to provide natural habitats for native wildlife as well as interpretive signage for environmental education.
- Locate a band shell within the park to play host to special events, concerts, summer theatres, etc., providing a social and cultural gathering space within the core area.
- Ensure accessible pedestrian links within and into the park.



# master design strategy + guidelines

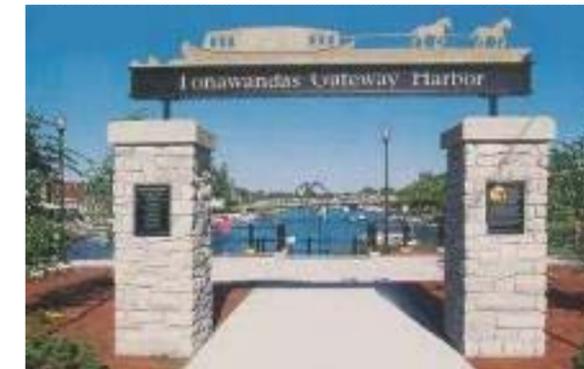
## 4.2.5 Memory Tree Park Improvements

- Enhance the water's edge, including clean-up and regular maintenance.
- Ensure pedestrian links within and to the park from the core area and residential areas.
- Provide areas for enjoying the views and vistas and personal reflection.



## 4.2.6 Defined Signage System

- Locate gateway signage at the intersection of Lansdowne Street and Highway 94 to provide a sense of arrival to the Downtown Waterfront Area, as well as along Main Street at the north and south limits of the core area and potentially at the foot of Lansdowne Street at the water's edge.
- Provide enhanced landscaping surrounding the gateway signage to add visual interest and aesthetic qualities.



#### 4.2.7 Potential Redevelopment and Infill Sites

A number of key sites within the Downtown Waterfront Area that have redevelopment potential, in conformity with the goals of the Master Plan, have been identified. The downtown represents a functional service centre with a range of civic and commercial services. In addition, the local public school and playing fields are within a short walk of the Lansdowne and Main Street intersection, while the waterfront and two public parks north and south of Lansdowne Street provide recreational opportunities for residents and visitors alike. It is within this context of a multi-functional service centre that the redevelopment opportunities should be viewed. A key to the future success of Downtown Callander is the provision of lands which best suit the needs of investors in commercial development – good location, capable of development at sufficient scale, potential for sufficient parking both on and off-street, and proximity to other public services. Figure 7 illustrates potential development / redevelopment sites within the Downtown Waterfront Area. Table 1, on page 26, provides details for each site, including current use and potential future uses.

- Include a broader range of uses than would be likely in other communities of similar size and which do not have the benefit of a significant range of tourism assets.
- The existence of the marina, and its future enhancement, the existence of the parks, as well as commercial retail, lend themselves to a moderate scale of development on key sites. Creating opportunities on these sites will likely require public investment in order to achieve the broader off-site amenities that are necessary to create the synergy of a more sustainable and attractive Downtown Waterfront Area.
- Achieving the right balance and scale of development on these sites is important to ensure that the overall character and aesthetics of Callander is maintained.
- Ensure that sites are realistic and scoped in the type of development opportunities which are pursued, given that overall demand for new commercial development is limited compared to larger centres such as North Bay, as well as seasonal considerations with respect to the elevated spending that occurs during the tourist season.
- Recognize that while some areas of the Downtown may not offer the most attractive sites or current uses, they offer important functions which should be maintained over time.



Figure 7: Potential Development / Redevelopment Sites



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**Table 1: Potential Development / Redevelopment Sites**

	Site	Location	Current Use	Potential Use
1	Former Red Line Hotel Site	Lansdowne Street east of Main Street	Vacant	<ul style="list-style-type: none"> <li>Higher density residential complex potentially with ground floor commercial uses. Given its location, higher density development for residential would appear appropriate.</li> <li>Niche opportunities catering to older households as well as the potential for senior's residences would appear to be an appropriate market opportunity.</li> <li>As an alternative to residential, this site provides opportunity for commercial development which can include retail but also commercial office space. It is noted that there is currently a lack of space available to meet the demands for office space from a variety of users including local medical/dental and other health related services.</li> </ul>
2	Royal Bank + Rental Property Sites	North west quadrant of Lansdowne and Main Street	Commercial / residential (rental apartments)	<ul style="list-style-type: none"> <li>The rental apartment site may itself provide the opportunity for infill residential development but alternatively could be developed for commercial development in keeping with the commercial development to the north and south.</li> <li>The extent to which the depth of the site could be fully used is a relevant consideration for any site planning exercise.</li> </ul>
3	Lot Adjacent to Callander wharf	In proximity to waterfront and marina, on north side of Lansdowne Street	Winter mooring of Chief Commanda II, unorganized parking area	<ul style="list-style-type: none"> <li>Opportunity for the development of a public space with services to provide an important focus for gathering and activity on a seasonal basis and additional parking.</li> <li>Further north, the opportunity exists for higher end residential development. The compatibility of residential development and adjacent public space will require careful consideration in any site planning exercise. There are opportunities to take full advantage of the high quality location and views to and from this promontory of land.</li> </ul>
4	Area between plaza and Municipal offices	East side of Main Street, north of Lansdowne Street (north of parking associated with retail plaza)	Retail Commercial / undeveloped (at rear of property)	<ul style="list-style-type: none"> <li>While the front portion of the land serves an existing commercial function, the long-term opportunity is for a more comprehensive redevelopment of these commercial lands to provide a continuum of retail development north from the Main Street / Lansdowne Street intersection.</li> </ul>
5	Public Library Site	Catherine Street, west of Main Street	Institutional	<ul style="list-style-type: none"> <li>Redevelopment or adaptive re-use purposes for a combination of commercial and / or residential uses.</li> </ul>
6	Ball Field at Royal Canadian Legion Property	Highway 94 and Lansdowne Street (major gateway into Downtown Waterfront Area)	Institutional / Recreational	<ul style="list-style-type: none"> <li>As a signature "gateway" site in Callander, it is recommended that if this site were to become available it should be considered for infill / redevelopment for mixed use development (residential / commercial).</li> </ul>

#### 4.2.8 Potential Parking Areas

The provision of parking in the core area was identified by stakeholders and the community as a concern as the Downtown Waterfront Area develops. There are many opportunities for on-street parking in downtown Callander, particularly near the intersection of Main and Lansdowne Streets. On Main Street, potential locations for on-street parking have been identified on the west side of the street to the north and south of Lansdowne Street; the on-street parking to the south is fairly continuous. On-street parking along Main Street is advantageous in that it provides a buffer between the pedestrian realm and vehicular traffic. On Lansdowne Street, a number of parking areas have been identified on both the north and south sides of the street extending to M.T. Davidson Public School. Figure 8 illustrates potential parking areas within the Downtown Waterfront Area.

Four potential off-street surface parking sites have been identified north of Lansdowne Street. The first site is located behind the LCBO store with access from Main Street, between the LCBO store and adjacent residential property, and with access from First Street. The size of this potential surface parking area is fairly small.

A second potential surface parking lot is located between the Canada Post and Lucky 13 buildings, on the east side of Main Street. There is a large amount of parking potential for this lot with access directly from Main Street and from the east on Catherine Street. This site could potentially be connected to Callander Square's parking lot.

The third site is the northern most location identified within the Downtown Waterfront Area, and is located east of Main Street, directly across from the Callander Lakeside Medical Clinic building and north of the Municipal Office parking lot. Access to the site would be from Main Street and could be treated as an extension of the Municipal Office parking lot. The site is currently vegetated, however, the potential number of parking spots that may be located here is considered moderate. Due to the topography / slope in this area an alternative exit should be investigated at the north end of this site.

Finally, the fourth site is located at the foot of Lansdowne Street near the wharf. This area would primarily serve those who are visiting the marina or the proposed Waterfront Place development, and would ideally be short-term parking. With the additional waterfront facilities proposed, and due to expansion limitations next to the wharf area, there will be a need for additional parking areas to service the wharf and marina area. Due to its proximity to the wharf and boat launch area, the road right-of-way at the north end of First Street has the potential to be utilized for angled boat trailer parking.

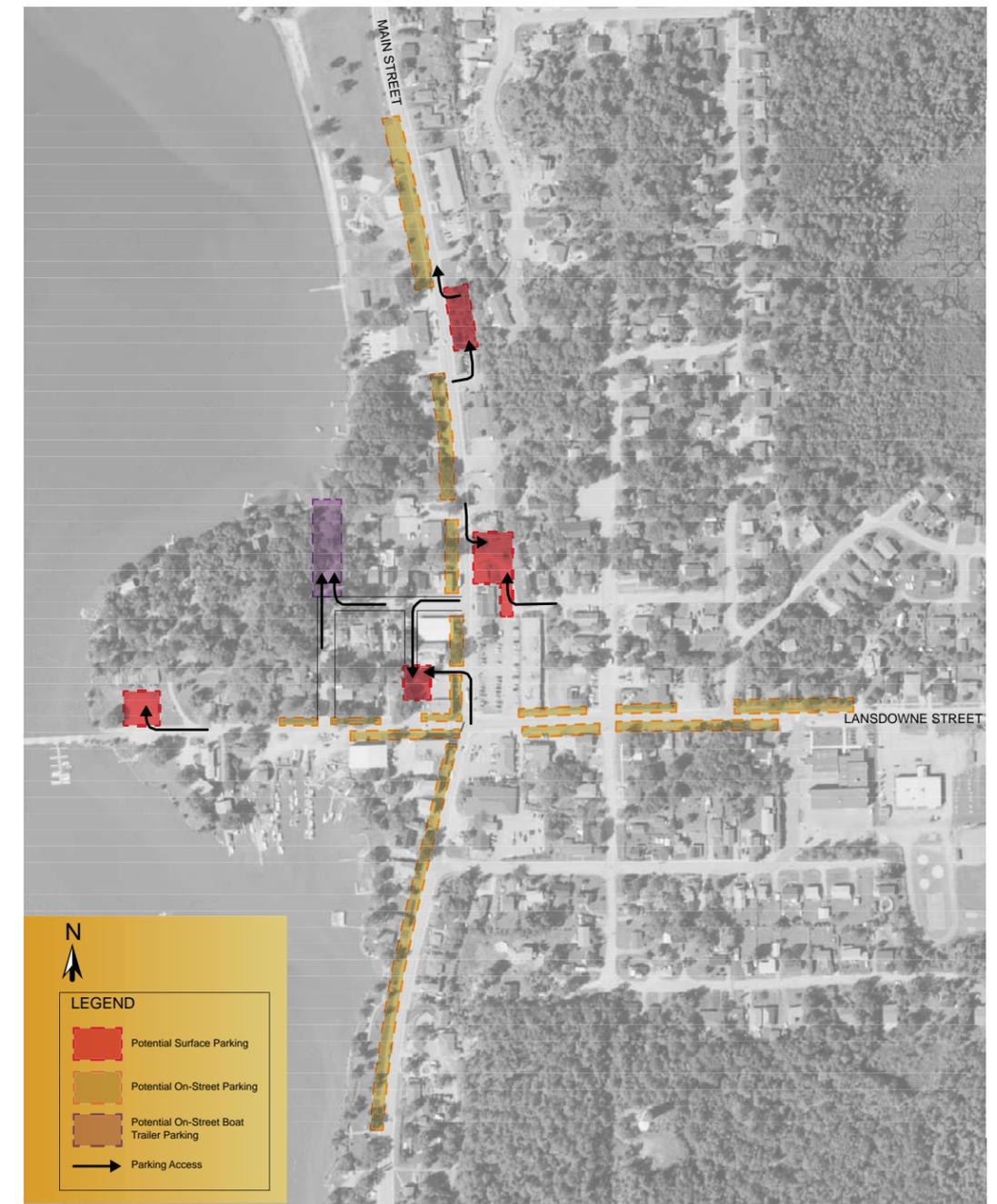


Figure 8: Potential Parking Areas

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## 4.2.9 Preferred Truck Route

Currently, there are a number of large trucks that travel along Main Street to and through the Downtown Waterfront Area. This causes excessive noise and traffic on the scenic road along the shoreline.

It is recommended that Lansdowne Street be identified as the preferred route for large trucks to access the commercial properties within the Downtown Waterfront Area. It is noted that Lansdowne Street is currently in the very early design phase of road reconstruction. This route will enable adequate access to the core area, while keeping truck traffic on Main Street to a minimum. The preferred truck route is illustrated in Figure 9.



Figure 9: Preferred Truck Route

4.3 Master Plan

**LEGEND**

-  DOWNTOWN AREA
-  COMMERCIAL CORE AREA
-  DEVELOPMENT ZONE
-  INSTITUTIONAL
-  RESIDENTIAL
-  PARK/OPEN SPACE
-  WETLAND/FISH HABITAT
- 1. Streetscape Improvements
- 2. Main Intersection Enhancements
- 3. Waterfront 'Place'
- 4. Potential Development Sites
- 5. Pier and Marina Improvements
- 6. Beach Enhancements: cleanup
- 7. Boat Launch Improvements
- 8. Event Parking/Ice Hut Staging Area
- 9. Park Improvements
- 10. Pedestrian Links
- 11. Protected Fish Habitat
- 12. Town Hall and Future Potential Library Location
- 13. Veterans' Ridge Park
- 14. Cenotaph property
- P Proposed Parking
-  Local/Residential Roads
-  Enhanced Pedestrian Route
-  On-road Bike Route
-  Defined Downtown Signage
-  Street Tree/Streetscape Enhancement

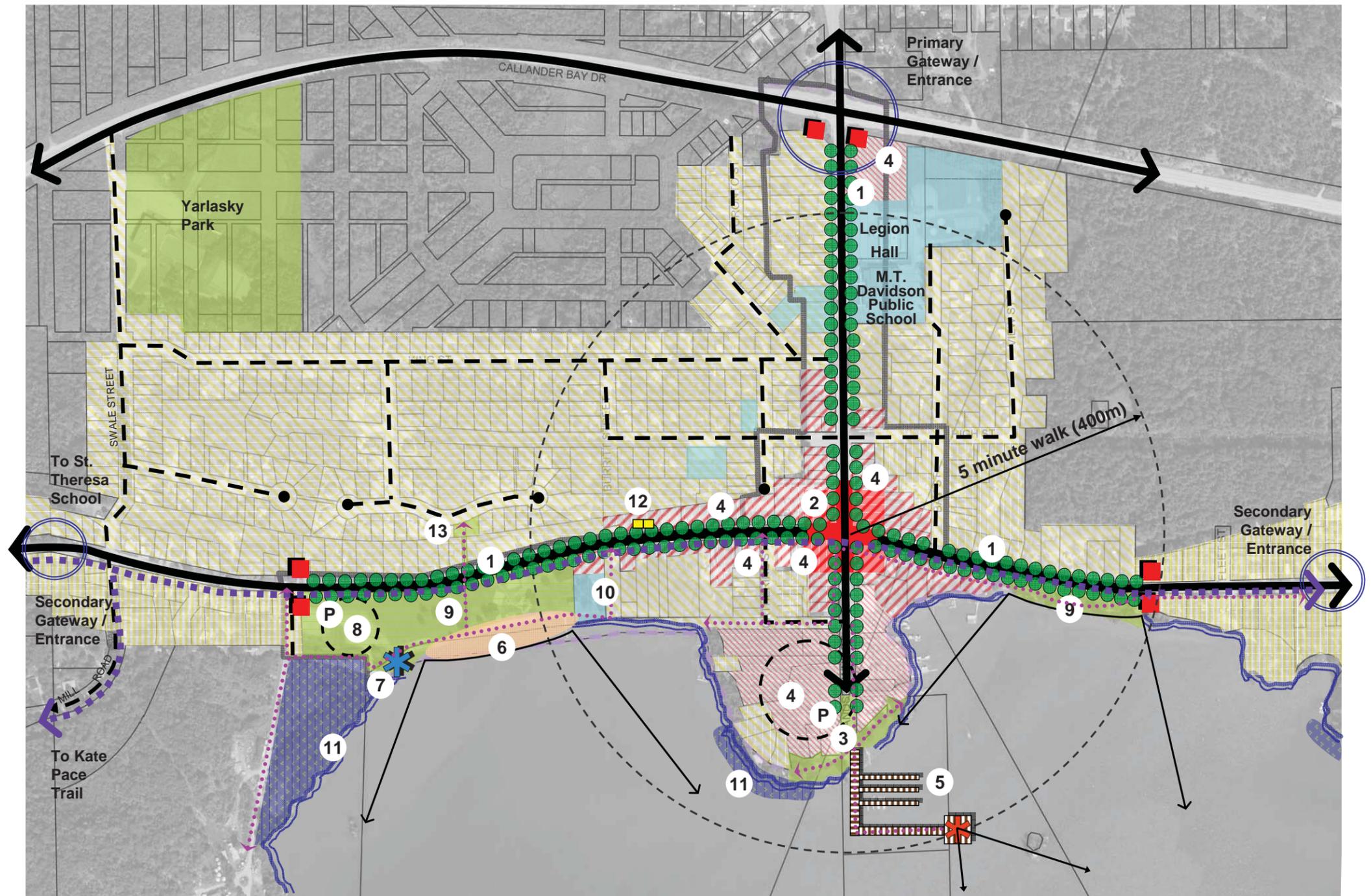


Figure 10: Master Plan Diagram

# master design strategy + guidelines

## 4.4 Design Themes

The most relevant design themes are those that have strong ties to the natural and cultural history of a place. There are many thematic opportunities that are apparent in the local landscape of Callander. Based on these opportunities unique to Callander, the Consulting Team has developed a preliminary list of key words and images providing a palette for the basis of developing design guidelines and themes for the Callander Downtown Waterfront Area.

These key words and imagery provides inspiration for the master plan and design guidelines relative to the selection of materials and character of the streetscape and built form within the Downtown Waterfront Area of Callander.

The primary design theme for Callander relates to the nautical, logging and lumbering heritage. Design elements have the potential to incorporate such components as heavy timber, bollards, stone / rocks, nautical flags, and naturalized waterfront areas.

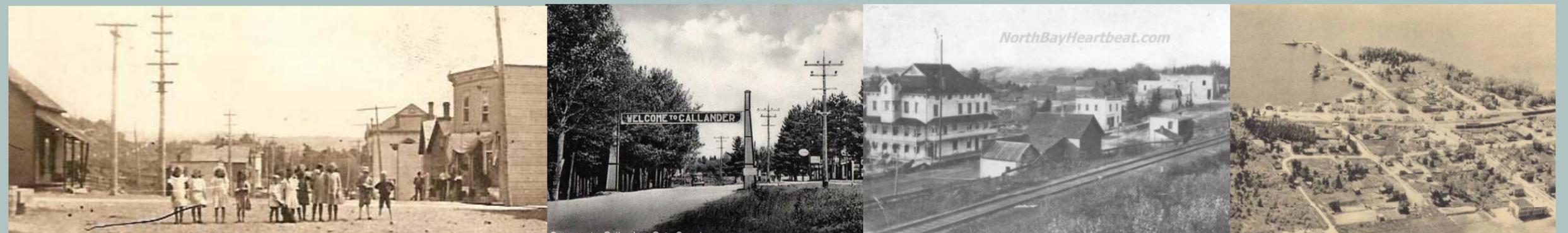
### **Natural Environment**

- Lake Nipissing
- Callander Bay
- Ecology



### **Local Heritage**

- Logging heritage
- Lumbering and Saw Mills
- Shipping and Nautical heritage



## 4.5 Streetscape Design

Developing an attractive streetscape design for Callander’s Downtown Waterfront Area will reinforce the current road system and improve the community’s overall quality of life. Well designed streetscapes can not only improve both vehicular and pedestrian circulation and interaction, and wayfinding and orientation, but also provide for public gathering and social spaces, encourage active transportation, promote the area as a commercial centre, and emphasize the image of Callander as an exciting and attractive community to live, work, shop and recreate.

This section of the report provides general streetscape design guidelines for all streets within the Downtown Waterfront Area. Streetscape elements that are important to the overall character of the street and general guidelines for these elements are provided. These guidelines are intended to create a pedestrian-oriented, aesthetically pleasing and well coordinated streetscape design that is complementary to adjacent land uses and built form.

Encouraging connections outside the study area is another element that is emphasized within this strategy. Bicycle lanes along Main Street and Lansdowne Street will help to link the Core Area with regional assets (including TransCanada Trail connections). That being said, any resurfacing contemplated by the Municipality and / or Ministry of Transportation (MTO) should have consideration for the inclusion of bicycle lanes.

The burying of overhead wires is recommended, at a minimum in the area surrounding the Main Intersection, to reduce visual clutter within the streetscape zone.

Winter conditions and snow clearing requirements should be considered when determining the final dimensions of the streetscape zone widths, as illustrated in Figure 11.



Existing Condition of Main and Lansdowne Streets



Downtown Waterfront Area Street Map

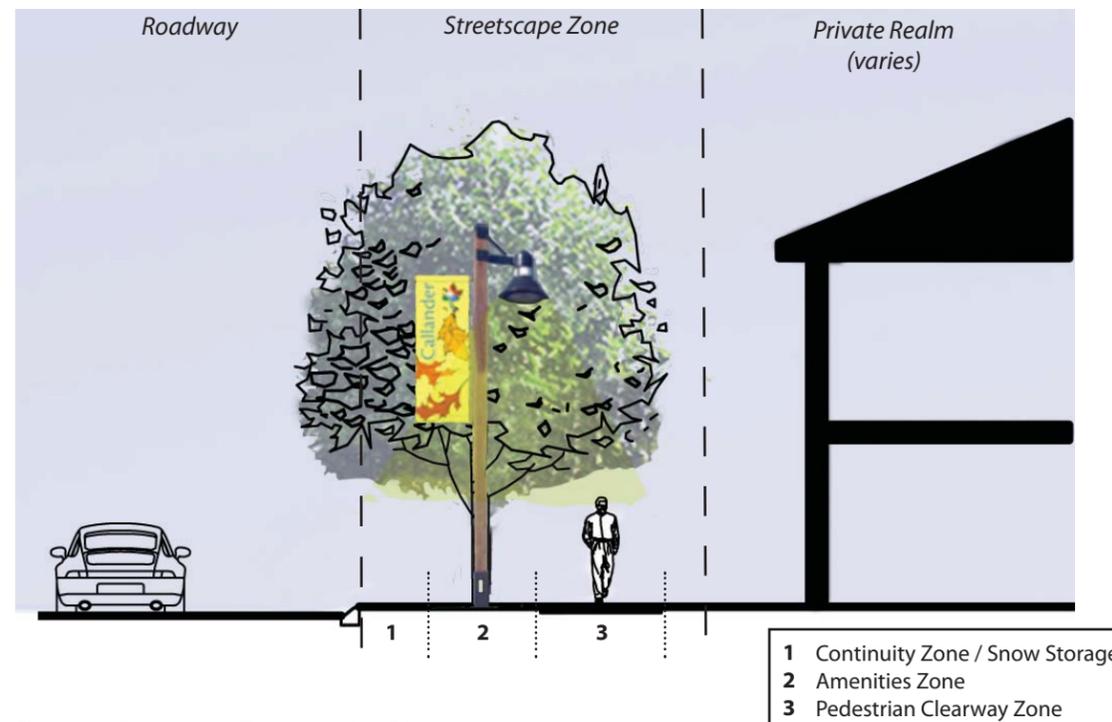


Figure 11: Streetscape Zone Location Diagram



# master design strategy + guidelines

## 4.5.1 General Guidelines

The streetscape zone is that area between the building face and the curb of the roadway. The streetscape shall be designed with consideration for the elements of the roadway as well as the adjacent built form or land uses. The placement and selection of streetscape elements shall have consideration for winter conditions, snow operations and snow removal techniques. The design and materials selected for streetscape elements shall reflect the nautical and logging heritage design theme through the use of wood and metal.

### Streetscape Elements

Developing a coordinated family of streetscape elements is important to convey the overall character and identity of Callander. Ensuring the safety, comfort and accessibility of pedestrians, cyclists and motorists are essential considerations of streetscape design. These elements shall provide a unified design to the streetscape zone, enhancing the comfort of the pedestrian realm, and providing space for social gathering.

Streetscape elements include street lighting and site furniture.

### Street Lighting

Street lighting provides safety for both vehicles and pedestrians, and is functional as well as aesthetic. The design of street lighting shall be consistent with the overall identity and character of the Downtown Waterfront Area, and shall provide adequate lighting for the roadway for vehicles and cyclists as well as the sidewalk zone for pedestrians.

#### Guidelines:

- Street lighting shall be in line with street tree planting, where appropriate;
- Specialty light fixtures may be used along Main Street and Lansdowne Street to highlight this area;
- Light standard styles, materials and types shall be coordinated with site furnishings to provide consistency along streetscape, incorporating natural wood and metal materials;
- Light standards should be designed to accommodate pedestrian lighting as well as street banners;
- All street lighting shall be dark sky friendly.

Luminaire selection will be determined based on street lighting level requirements and design standards relative to height, spacing and intensity levels. Figure 12 illustrates the lighting plan concept and light pole design themes.

# Lighting Plan Concept

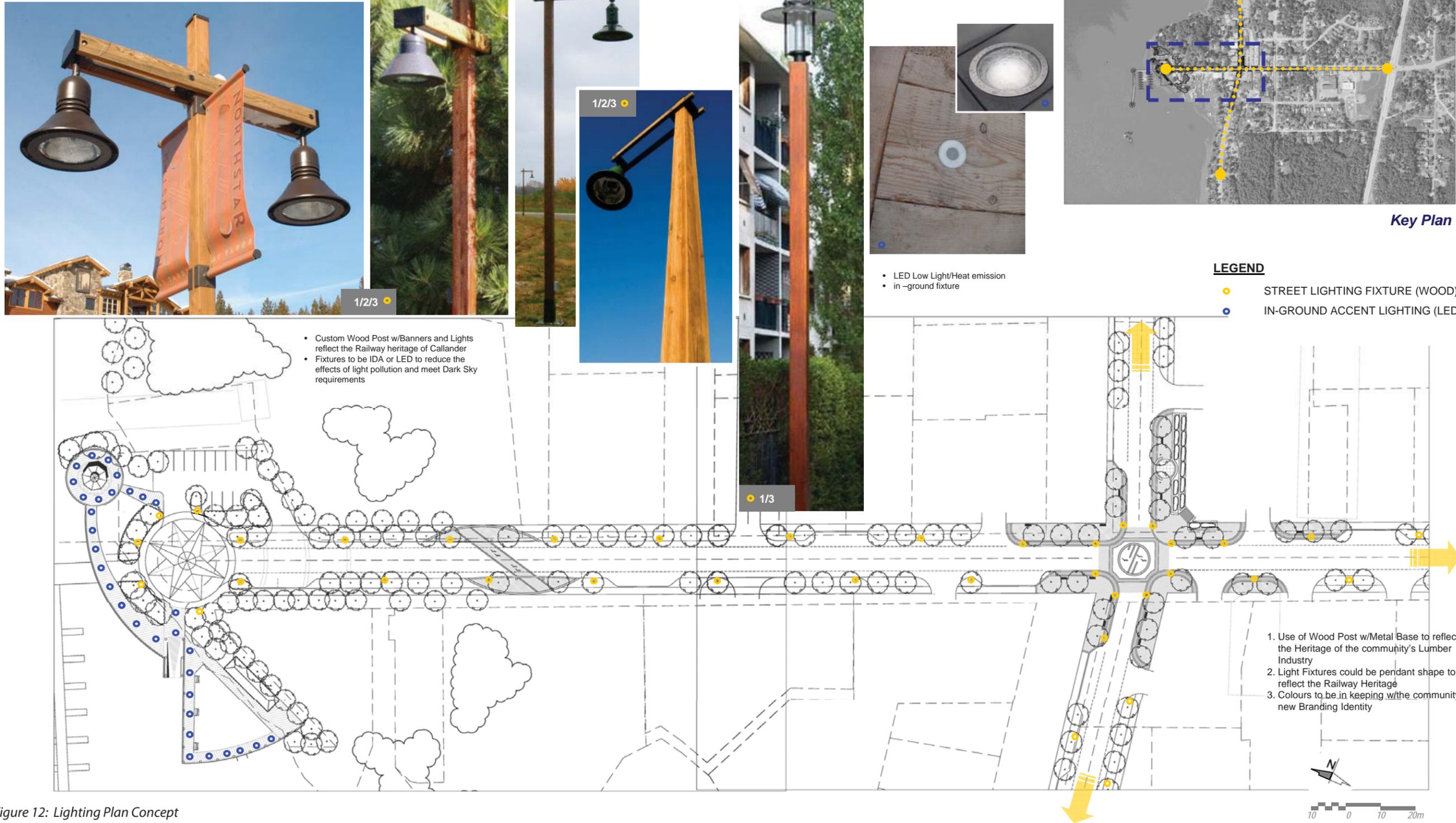


Figure 12: Lighting Plan Concept



# master design strategy + guidelines

## Site Furnishings

Site furniture will unify and enhance the streetscape and landscape elements within the pedestrian realm. These furnishings shall be consistent in design and provide function as well as aesthetic appeal to reinforce the overall nautical and lumbering heritage theming for the streetscape. The appropriate placement of attractive street furniture will reinforce the design intent, enhance the public realm and improve the overall use of public space. Site furnishings include benches, waste and recycling receptacles, bicycle racks, and planters.

### Guidelines:

- Colours, finishes, types and styles of site furniture shall be complementary and reinforce the overall identity and theme for Callander;
- Site furniture placement and arrangement should encourage overall safety and efficient snow removal considerations, as well as strengthen the streetscape;
- Furnishings should be tied together by a common colour palette and theme to avoid visual clutter;
- All street furniture should be low maintenance, vandal resistant, and easily replaceable;
- Some furniture may have the potential for seasonal removal to facilitate snow storage and removal.

Figure 13 illustrates the furniture plan concept and furniture design themes.

## 1. Benches

- Benches shall be durable and should have a back and arm rests for user comfort;
- Benches shall utilize natural wood / metal materials to enhance the overall nautical and logging design theme;
- Benches should be approximately 6 feet in length to accommodate 2 to 3 people comfortably.

## 2. Waste / Recycling Receptacles

- Waste containers shall be durable;
- Containers should be animal proof, equipped with plastic liners, possess a rain proof top, and be easily accessible for emptying.

## 3. Bicycle Racks

- Bicycle racks shall have embedment mounting;
- Should be located near points of interest (Waterfront Place, Centennial Park, etc), attractions as well as within the core commercial area (Main and Lansdowne Streets).

## 4. Planters

- Consistent design style with benches and waste receptacles, using wood / metal where possible;
- Plant material should be changed seasonally to provide visual interest and keep streetscapes aesthetically pleasing;
- Planters shall be located on sidewalk along Main and Lansdowne Streets within the core area.

# Furniture Plan Concept

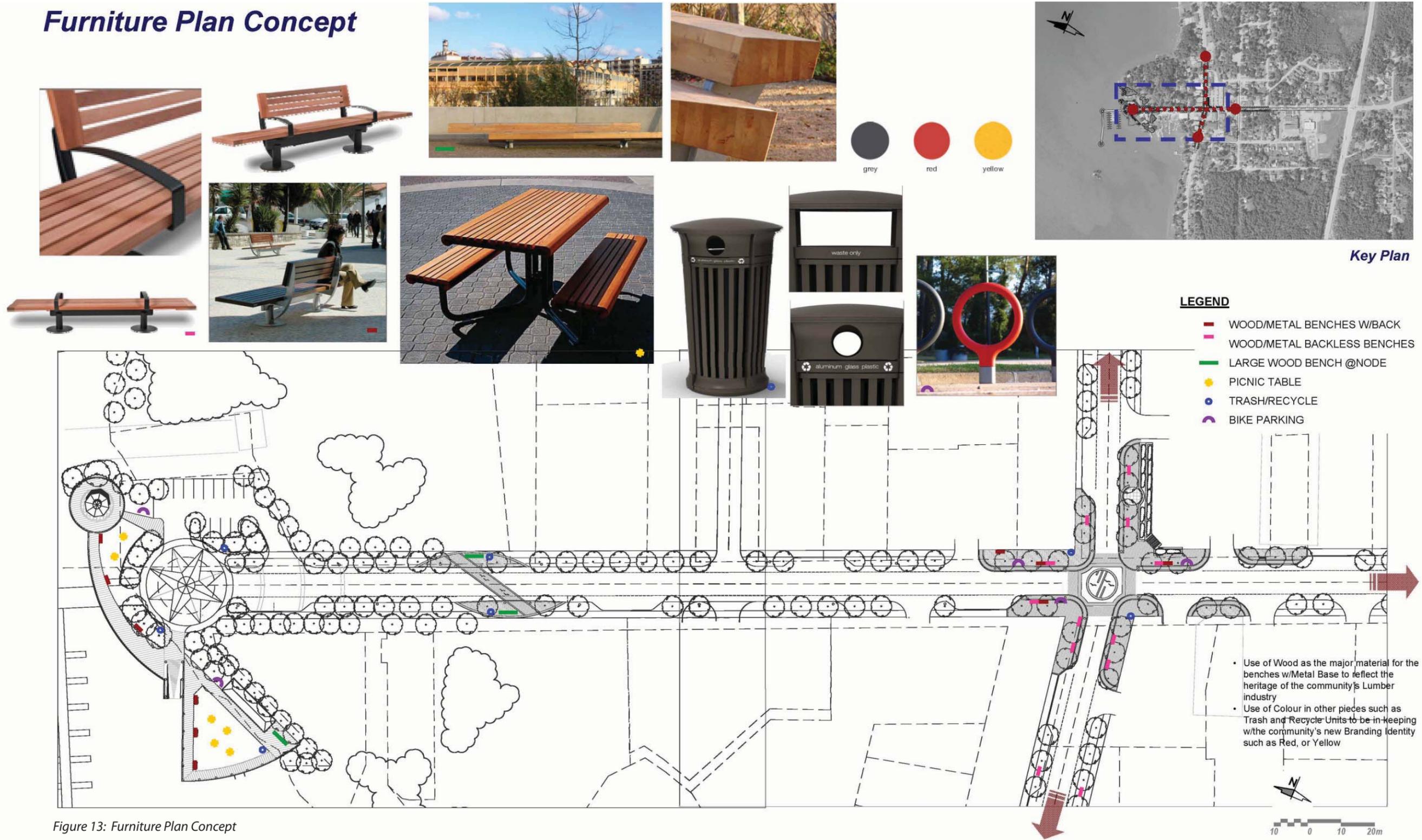


Figure 13: Furniture Plan Concept

# master design strategy + guidelines

## Sidewalks

Sidewalks of 2.0 metres minimum in width are recommended to be located in the following areas (where appropriate):

- Both sides of Lansdowne Street between Highway 94 and Waterfront Place;
- Both sides of Main Street between Memory Tree Park and Centennial Park;

Sidewalk width, shape or texture may vary in some locations to allow for various streetscape elements, gathering spaces, pedestrian activity nodes, etc. The use of specialty paving should be implemented at important intersections and / or pedestrian crosswalks to delineate the pedestrian realm. Sidewalks should assume a curvilinear shape (where appropriate) in order to preserve natural or cultural features that are present within the community.



## Street Trees

Street trees provide both a functional and aesthetic value to the overall character of the streetscape. Trees play a unifying role in site development and creates a distinction among streets.

### Guidelines:

- A row of street trees should be planted between the sidewalk and the curb, as appropriate. Variations may occur in response to adjacent land uses such as open space, points of interest, or community spaces. The overall objective is a continuous tree canopy along both sides of the street;
- Use species that are relatively low maintenance and non-invasive;
- Preference of street tree should be given to hardy native, salt-tolerant species;
- Trees should be planted in groupings to create street blocks and at special places and intersections, rather than as single specimens;
- Tree spacing should be approximately 8 -10 metres on centre;
- Street lighting and utility boxes should be placed in line with street trees to avoid visual clutter, unless otherwise specified.

Figure 14 illustrates streetscape special features, gateways, paving and banner details and design themes.

### Special Features/Paving/Banners



1



1



3



2



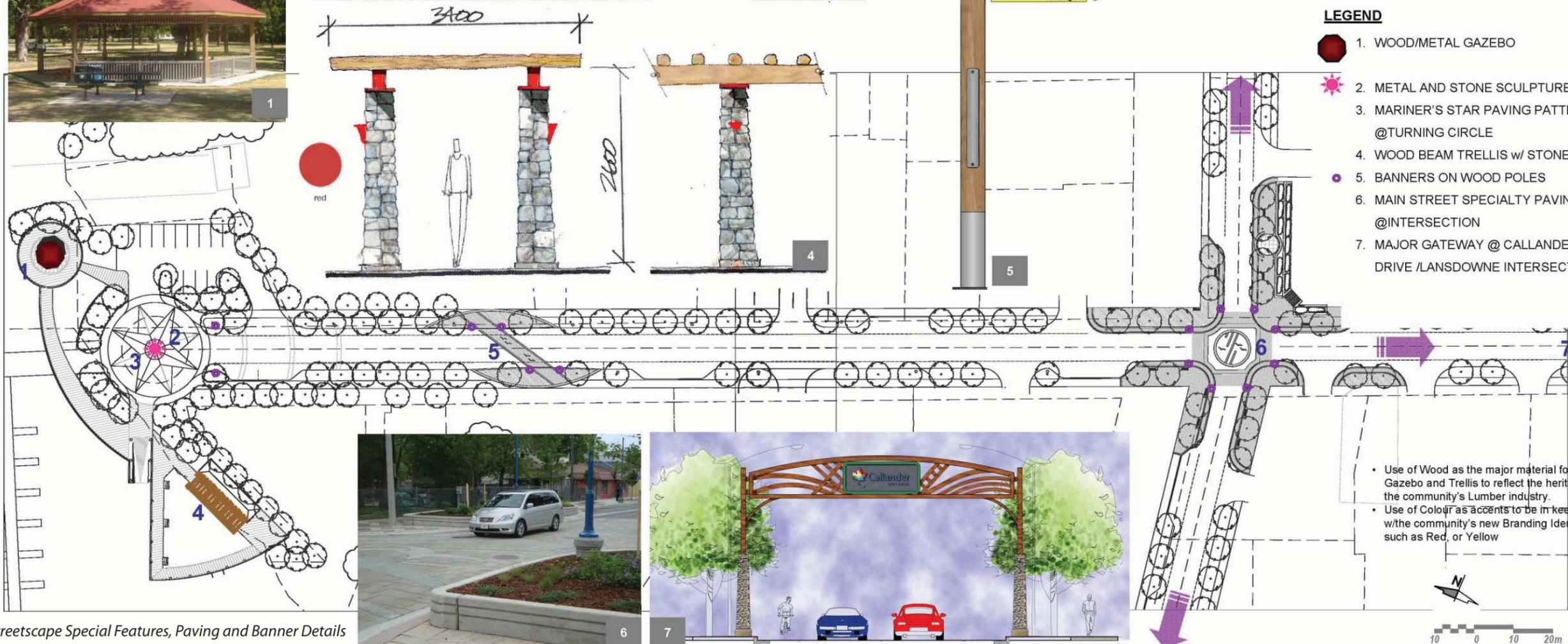
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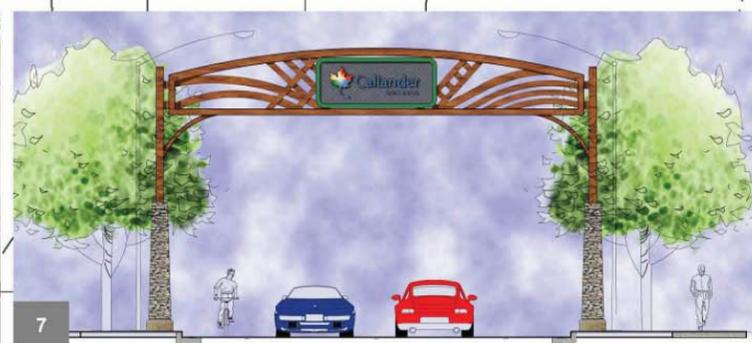
Key Plan

**LEGEND**

- 1. WOOD/METAL GAZEBO
- 2. METAL AND STONE SCULPTURE
- 3. MARINER'S STAR PAVING PATTERN @TURNING CIRCLE
- 4. WOOD BEAM TRELLIS w/ STONE BASE
- 5. BANNERS ON WOOD POLES
- 6. MAIN STREET SPECIALTY PAVING @INTERSECTION
- 7. MAJOR GATEWAY @ CALLANDER BAY DRIVE /LANSDOWNE INTERSECTION



4



7

- Use of Wood as the major material for the Gazebo and Trellis to reflect the heritage of the community's Lumber industry.
- Use of Colour as accents to be in keeping w/the community's new Branding Identity such as Red, or Yellow

Figure 14: Streetscape Special Features, Paving and Banner Details

# master design strategy + guidelines

## Signage

Upgrades to the current signage scheme will create a more significant arrival into Callander and provide orientation and wayfinding to pedestrians, cyclists and vehicles. A hierarchy of signage components provides tourists, visitors and residents to easily locate attractions and points of interest within Callander.

### Guidelines:

- Signage within the Downtown Waterfront Area shall be consistent in terms of overall design and lettering;
- Signage should incorporate community identity features such as the Municipal logo and community name on appropriate signs;
- Signage shall incorporate the nautical and logging heritage theme, using materials such as wood timbers, natural stone / rocks, rustic metal, etc.;
- Guidelines shall apply to all elements, such as park signs, trails signs and markers, environmental signage and educational / interpretive signage.

Refer to Figure 15 for recommended locations of gateway signage.



Inspiration: Historic Callander Gateway



## Gateway Signage

### Primary Gateway / Entrance Signage

A community gateway sign on Lansdowne Street at Highway 94 will provide a distinct sense of place and arrival for those entering the Downtown Waterfront Area of Callander from Highway 94. This sign is recommended to convey the nautical and logging heritage of Callander using natural wood, stone and metal.



### Secondary Gateway / Entrance

Core area gateway signage should be placed on the north and southern limits of Main Street, to provide orientation towards the commercial core area of Callander. This signage should reflect the cultural heritage of Callander with native planting and landscaping to provide visual interest.



### Downtown Core Area Gateway / Signage

The opportunity for a major pedestrian gateway exists within Callander guiding people to the waterfront and other attractions. This gateway sign shall provide a clear sense of direction and purpose for visitors and residents to access special places.



**LEGEND**

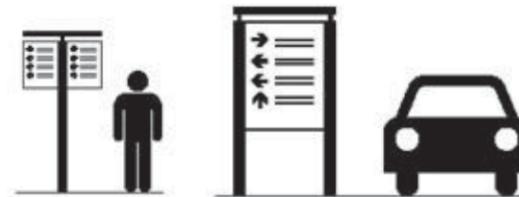
-  Gateway/Entrance Signage
-  Waterfront Gateway/Entrance Signage
-  Downtown Core Area Gateway/Signage
- 1. Yarlasky Park/Community Centre/ Bill Barber Complex
- 2. Legion Hall
- 3. M.T. Davidson Public School
- 4. Town Pier/Marina
- 5. Centennial Park
- 6. Veteran's Ridge Park
- 7. Memory Tree park

Figure 15: Community Gateway Locations Diagram

# master design strategy + guidelines



Existing Directional Signage at Main and Lansdowne Streets



## Wayfinding and Interpretive Signage

### Directional Signage

Directional signage can be very useful for both pedestrians and vehicles. This type of signage should be strategically located at major decision points throughout Callander.

### Interpretive Signage

With Callander's extensive natural and cultural history there is great opportunity to introduce interpretive signage elements throughout the Downtown Waterfront Area. These signs may include information related to the nautical, shipping and logging history, the volcanic history or the natural environment.

Refer to Figure 16 for recommended locations of directional and interpretive signage.



Nautical Themed Signage



## Points of Interest

1. Yarlasky Park/Community Centre/  
Bill Barber Complex
2. Legion Hall
3. M.T. Davidson Public School
4. Town Pier/Marina
5. Centennial Park
6. Veteran's Ridge Park
7. Memory Tree park



**LEGEND**

Gateway/Entrance Signage

Waterfront Gateway/ Entrance Signage

Downtown Core Area Gateway/Signage

Directional Signage

Interpretive Information

**North**

- Centennial Park
- Yarlasky Park/Community Centre/ Bill Barber Complex
- Small Boat launch
- Library/Town Hall

**East**

- Royal Canadian Legion Hall
- M.T.Davidson Public School
- Church
- Callander Bay Heritage Museum & Alex Dufresne Gallery

**South**

- Memory Tree Park
- South Shore Community Centre
- Sunshine Beach
- Powassan Cycle Route
- Nipissing Beach

**West**

- Town Pier/Marina
- Boat Launch
- Promenade
- Tour Boat Dock

Downtown Shops  
 • Library/Town Hall  
 • Callander Bay Heritage Museum & Alex Dufresne Gallery

Yarlasky Park/Community Centre/  
 Bill Barber Complex  
 • Shannondale Trail

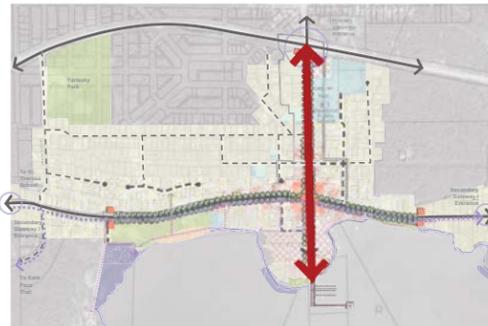
Kate Pace Trail  
 • Cranberry Trail  
 • Callander Wetland Preserve

Figure 16: Wayfinding and Interpretive Signage Locations Diagram

# master design strategy + guidelines

## 4.5.2 Lansdowne Street

Lansdowne Street is the major east-west spine within the Callander Downtown Waterfront Area; running perpendicular to Highway 94 and the water's edge, this street creates a link between these two features. Lansdowne Street at Highway 94 is one of the key community gateway entrances into Callander. This street passes through the core commercial area and provides access to the wharf / pier and proposed Waterfront Place development. Lansdowne Street has been identified as the preferred truck route for accessing the Downtown Waterfront Area. Figure 17 illustrates the detail plan for Lansdowne Street.



Key Plan



Figure 17: Detail Plan - Lansdowne Street

### Features:

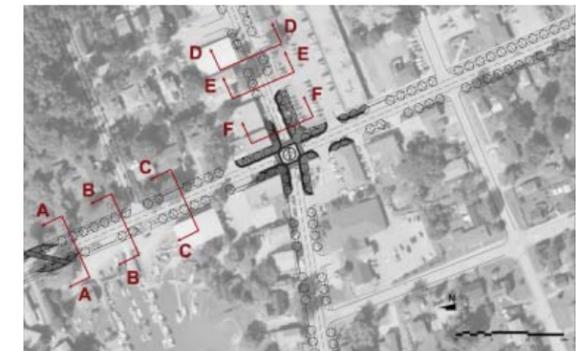
- 20 metre right-of-way;
- 2 vehicular lanes of 3.5 metres width in each direction;
- 2 bicycle lanes of 1.75 metres width in each direction;
- 2.75 metre boulevard on each side of street for lighting, street tree planting and street furniture;
- 2.0 metre wide sidewalk on each side of street;
- On-street parking will occur in lay-by parking bays, where appropriate. In these locations parking shall be located within the boulevard (see Section C-C).

### Character:

- Residential / Commercial / Open Space interface;
- Strong gateway design features at east and west limits - Gateway design features, specialty paving, boulevard planting;
- Pedestrian comfort shall be ensured - Shade and wind protection and shelter.

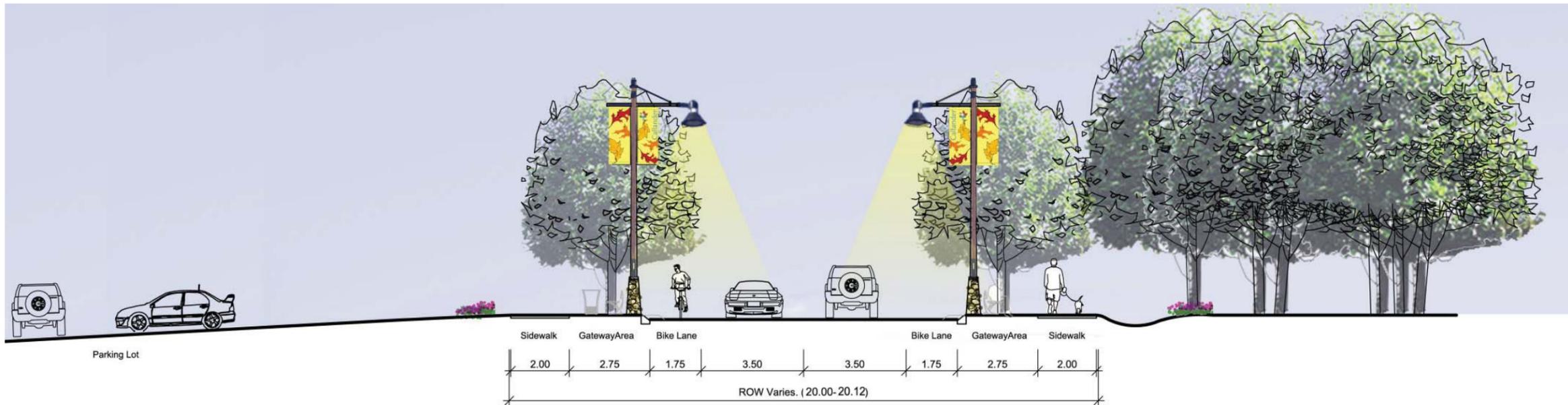
### Guidelines:

- Sidewalks should be provided on both sides of the street, providing access to the commercial areas, residential areas, open space and the public school and Royal Canadian Legion located near Highway 94;
- Decorative street lighting with banners integrated with poles;
- Decorative and specialty paving may be used here to delineate gateway features and entrance into the commercial core;
- Benches are recommended to be provided along Lansdowne Street;
- Design materials, colours and architectural styles should be consistent with the overall design vision for the Callander Downtown Waterfront Area.



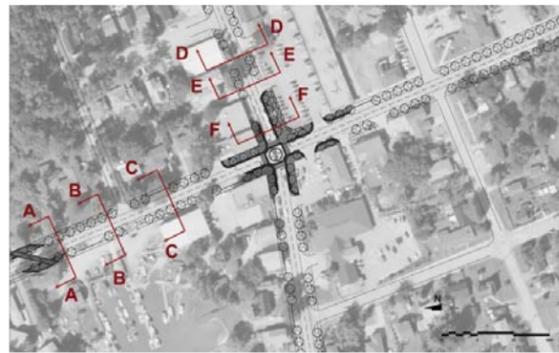
Section Key Plan

Current Condition of Lansdowne Street Near Pier



SECTION A-A: Lansdowne Street at Waterfront Gateway

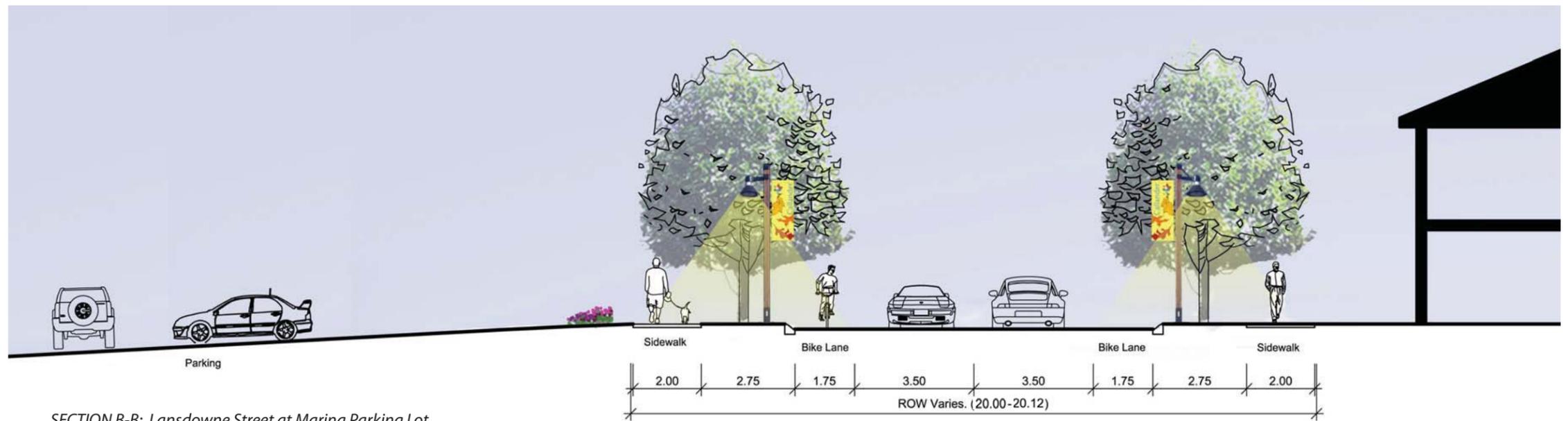
# master design strategy + guidelines



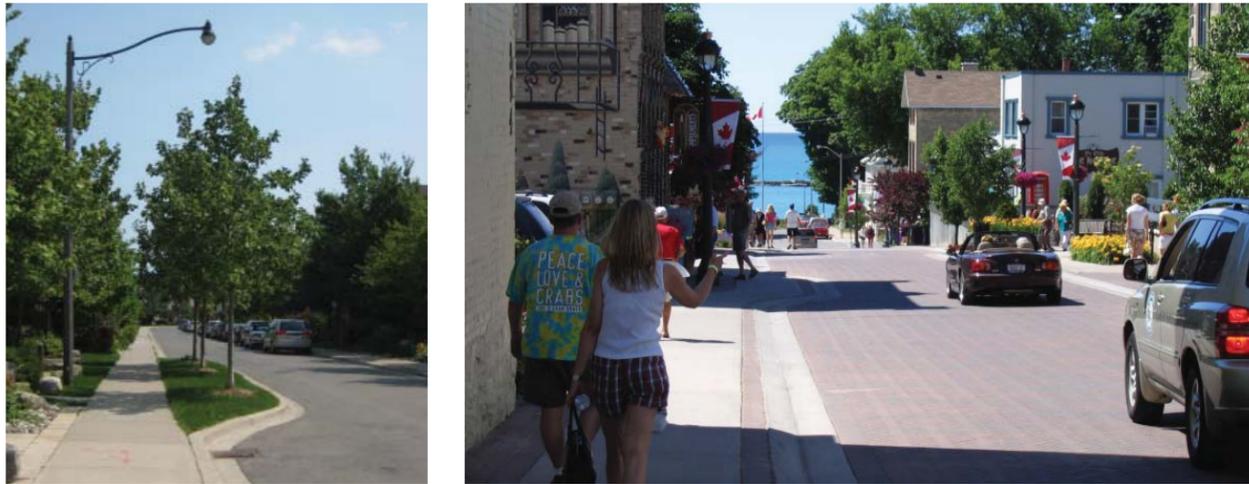
Section Key Plan



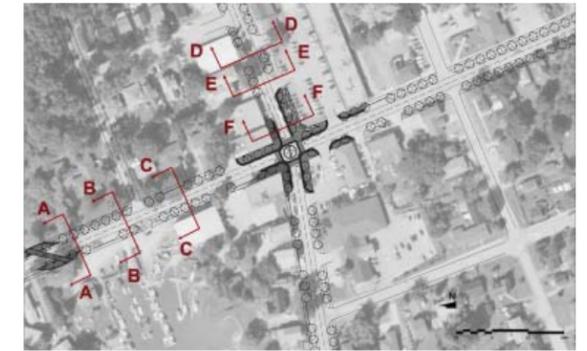
Precedent Imagery



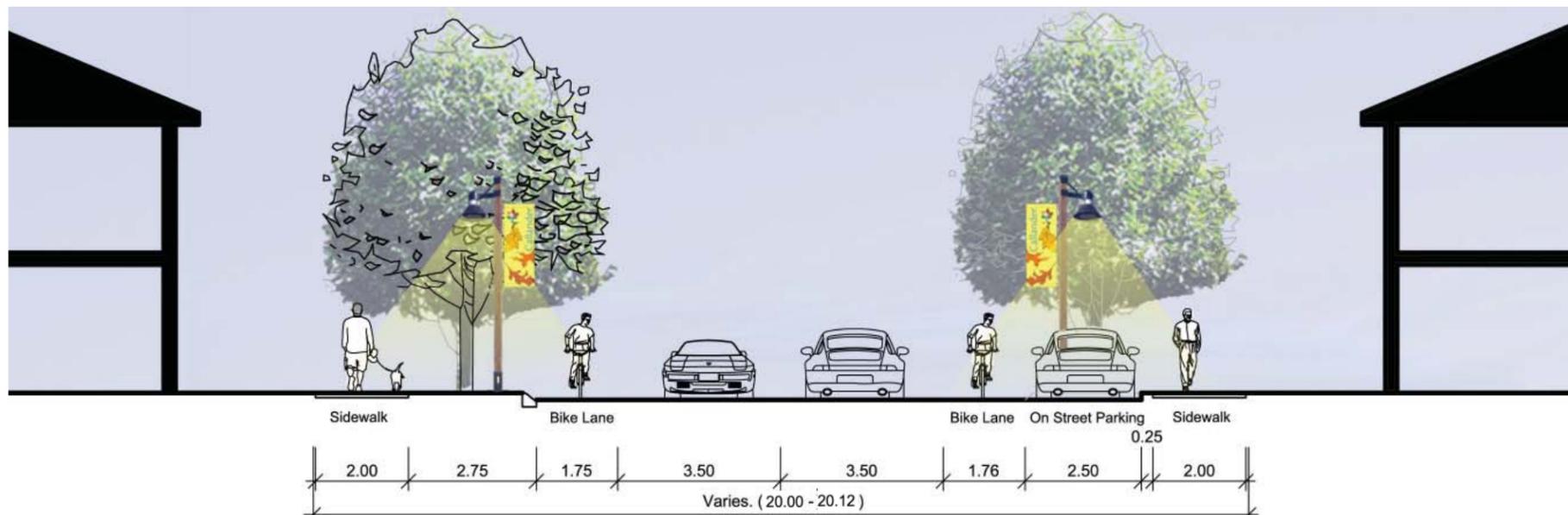
SECTION B-B: Lansdowne Street at Marina Parking Lot



Precedent Imagery



Section Key Plan



SECTION C-C: Lansdowne Street with Residential / Commercial Uses and On-Street Parking

# master design strategy + guidelines

## 4.5.3 Main Street

Main Street is the major north-south spine within the Callander Downtown Waterfront Area, running parallel to Highway 94 and the water's edge. Main Street runs between Highway 94, creating gateway opportunities at the north and south limits of the Downtown Waterfront Area. This street passes through the core commercial area and provides access to many commercial and residential properties. Figure 18 illustrates the detail plan for Main Street.



Key Plan

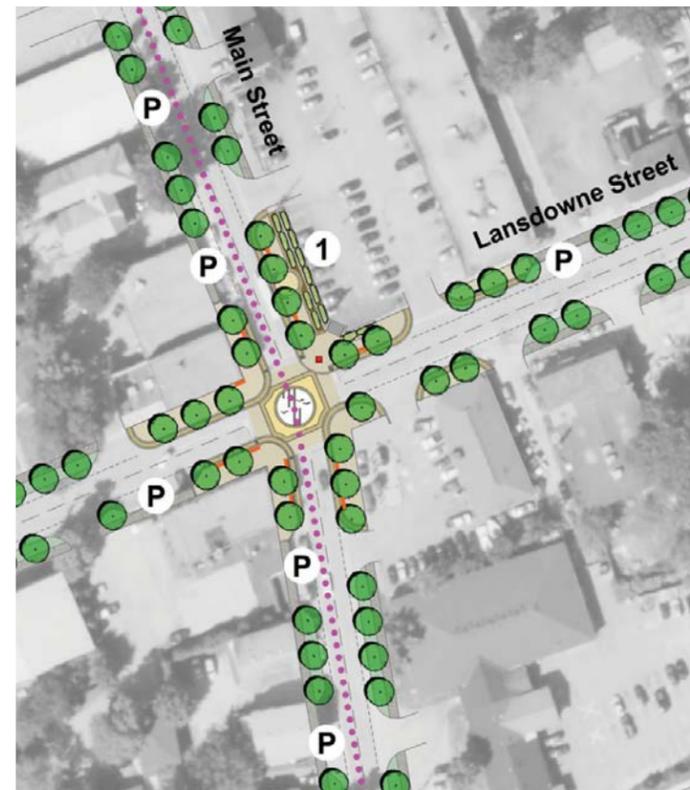


Figure 18: Detail Plan - Main Street

### Features:

- 20 metre right-of-way;
- 2 vehicular lanes of 3.5 metres width in each direction;
- 2 bicycle lanes of 1.75 metres width in each direction;
- 2.5 metre boulevard on each side of street for lighting, street tree planting and street furniture;
- 2.0 metre wide sidewalk (minimum) on each side of street;
- On-street parking will occur in lay-by parking bays, where appropriate. In these locations parking shall be located within the boulevard (see Section D-D).

### Character:

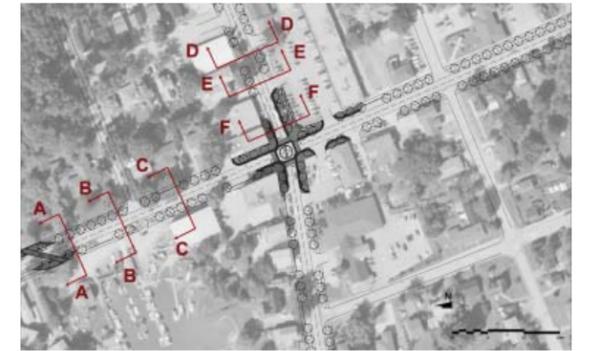
- Commercial / Civic / Open Space interface, with residential away from main intersection;
- Gateway design expression at north and south limits - Gateway design features, specialty paving, boulevard planting;
- Pedestrian comfort shall be ensured - Shade and wind protection and shelter.

### Guidelines:

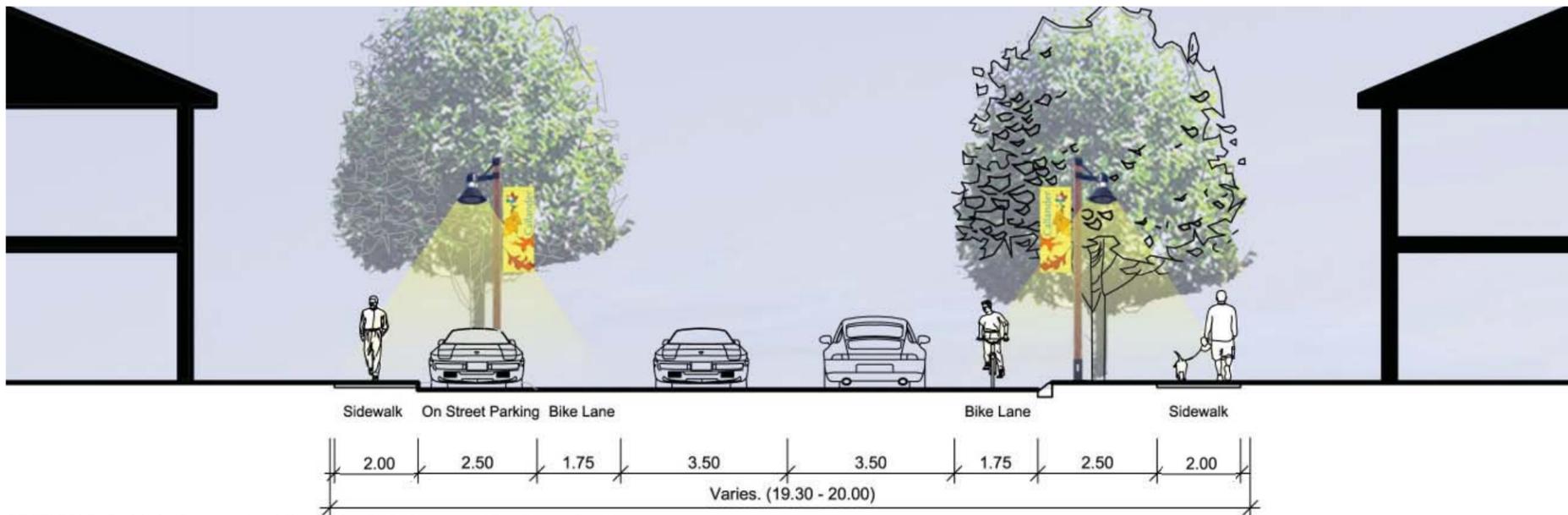
- Sidewalks should be provided on both sides of the street, providing access to the commercial properties, residential areas and open space;
- Decorative street lighting shall enhance the character of the commercial core;
- Decorative and specialty paving may be used here to delineate gateway features, pedestrian crossings at park and entrance into the commercial core;
- Benches are recommended to be provided along Main Street;
- Design materials, colours and architectural styles should be consistent with the overall design vision for the Callander Downtown Waterfront Area.



Current Condition of Main Street

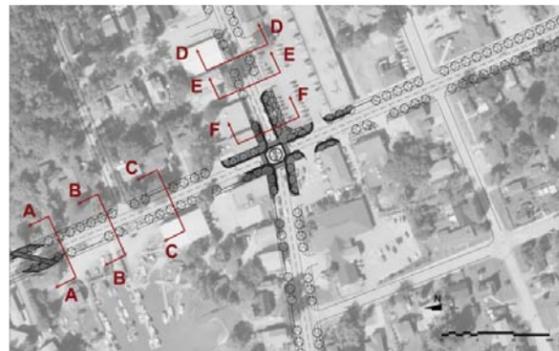


Section Key Plan



SECTION D-D: Main Street with Mixed-Use Buildings and On-Street Parking

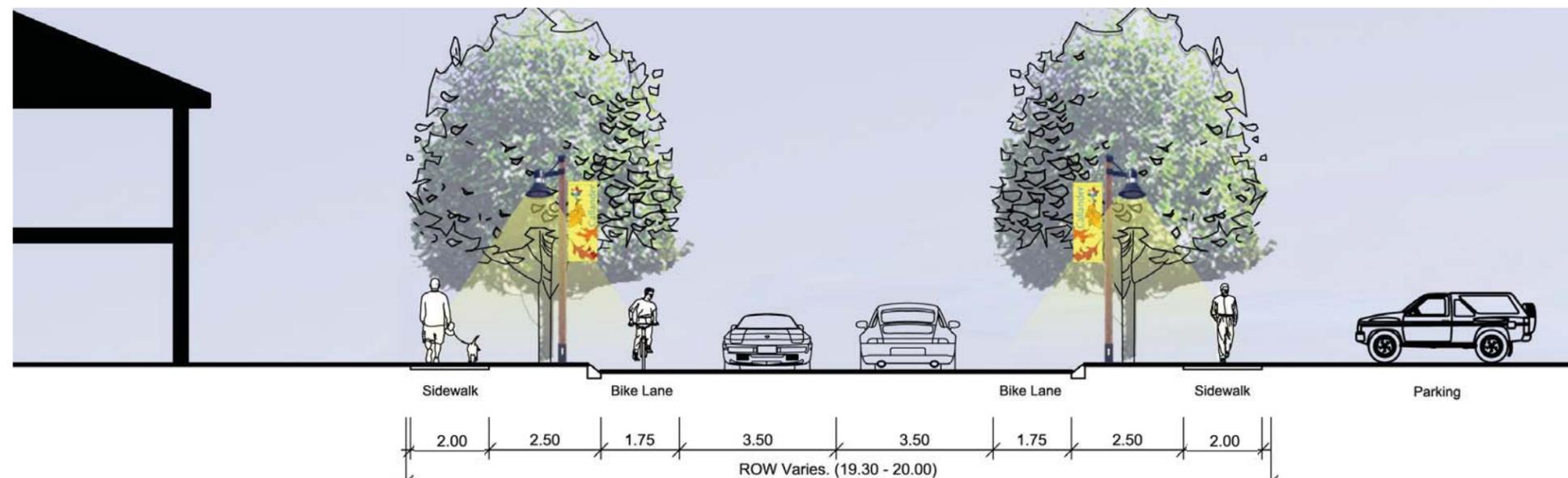
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Section Key Plan



Precedent Imagery



SECTION E-E: Main Street with Mixed-Use Buildings and Surface Parking Lot

### 4.5.4 Intersection Improvements

The intersection of Lansdowne and Main Streets is in the centre of the commercial core of Callander. This intersection should announce your arrival into the core area and provide a sense of place and belonging for residents, visitors and tourists. Creating a pedestrian-friendly intersection through seating, enhanced landscaping, specialty paving, enhanced pedestrian crossings, and signage and wayfinding will define this area as one of the key focal points within the Downtown Waterfront Area. A signature parkette is to be located on the north east corner of the intersection to provide a gathering space for social interaction (see Figure 19 for intersection improvements detail plan).

*Features:*

- 20 metre right-of-way;
- 2 vehicular lanes of 3.5 metres width in each direction;
- 4.4 metre wide sidewalk with 1.6 metre wide boulevard on west side of Main Street;
- 3.0 metre wide sidewalk with 3.0 metre pedestrian boulevard area on east side of Main Street (at signature parkette - see Section F-F).

*Character:*

- Commercial / Open Space interface;
- Signature design expression - design features, specialty paving, boulevard planting, signage and wayfinding;
- Pedestrian comfort shall be ensured - shade and wind protection and shelter.

*Guidelines:*

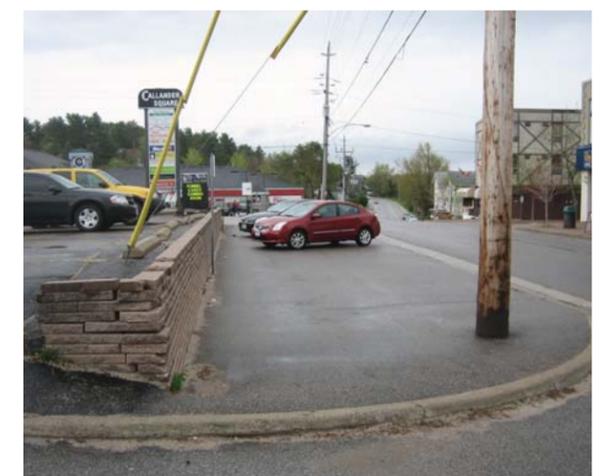
- Sidewalks should be provided on both sides of the street, providing access to the commercial properties and open space;
- Decorative and specialty street lighting shall enhance the overall character of the commercial core;
- Decorative and specialty paving may be used here to delineate signature parkette feature, intersection and pedestrian crossings, and entrance to the plaza;
- Benches are recommended to be provided in the areas surrounding the intersection as well as in the signature parkette;
- Design materials, colours and architectural styles should be consistent with the overall nautical and logging design theme for the Callander Downtown Waterfront Area.



Key Plan

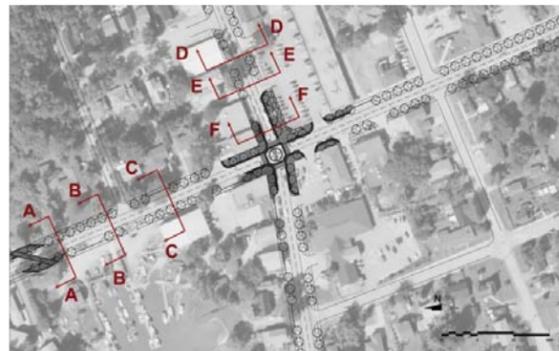


Figure 19: Detail Plan - Intersection Improvements



Current Condition of North East Corner of Main and Lansdowne Streets

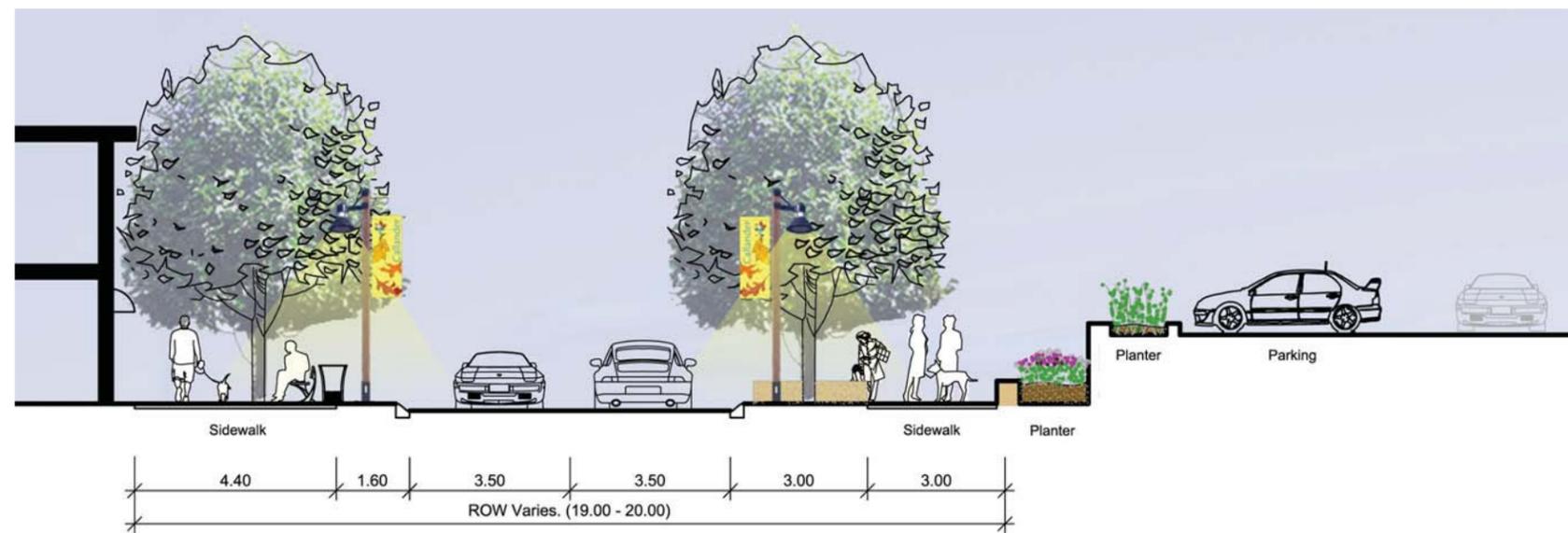
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Section Key Plan



Precedent Imagery



SECTION F-F: Main Street at Signature Parkette with Elevated Plaza Parking Lot

## 4.6 Parks and Open Space Design

The Callander Downtown Waterfront Area currently has four parks and open spaces, including:

1. Centennial Park
2. Veteran’s Ridge Park
3. Memory Tree Park
4. Cenotaph Property

Centennial Park is located on Main Street North, north of Lansdowne Street. It is a waterfront property with a number of facilities including children’s playground, picnic area, small concession stand, beach area, and boat launch. The opportunity to upgrade existing facilities and possibly add new facilities was an issue that was raised by many during the consultation process.

It is also important to mention the pier (Callander wharf), which is in Municipal ownership, as this could also provide an additional open space for the community to enjoy. It is recognized that the pier is in need of extensive repair or rebuilding, however, there is the opportunity to provide upgrades to this area and create a waterfront destination on Callander Bay.

There is the opportunity to link the parks within the Downtown Waterfront Area with other waterfront facilities, the core commercial area, local schools, Yarlasky Park, Bill Barber Complex, South Shore Community Centre and regional linkages to North Bay via the Kate Pace Way. The opportunity also exists to develop the Cenotaph property (currently under utilized), adding shade trees, benches and floral displays to create an attractive year-round passive area.

This section of the report provides a description and set of design guidelines for the further development of Centennial Park as well as the Waterfront Place, located in the Municipally-owned lands adjacent to the pier. Figure 20 illustrates the location of parks and open space within the Downtown Waterfront Area.



Figure 20: Location of Parks and Open Space within Study Area



Current Condition of Centennial Park



Current Condition of Memory Tree Park

# master design strategy + guidelines



Key Plan

## 4.6.1 Centennial Park

Centennial Park, located between Main Street and the water's edge will continue to be a central feature within the Callander Downtown Waterfront Area. It will provide residents and visitors with a number of exciting upgraded and new elements throughout the year. These elements include playground improvements, water play area, grass amphitheatre with band shell, waterfront promenade, enlarged beach area, overall beach enhancements, boat launch improvements for non-motorized boats, stream re-naturalization, and re-organized parking area.

As a signature waterfront park, Centennial Park serves both the community and visitors alike. As such, it will be developed using best practices for sustainable

development. There are numerous benefits associated with restoring a more naturalized shoreline, including the protection of shorelines, reduced erosion and nutrient loading, re-established wildlife habitat, improved fish habitat and water quality, and is visually pleasing. Native species of shrubs and perennials shall be planted within Centennial Park; these may include Wild red current, Saskatoonberry, Native Dogwoods, Common wild rose, Russet Buffaloberry, Shrubby cinquefoil, Harebell, Canada Violet and Blue Flax.

Upgrades to the parking area include a revised parking layout, granular / permeable paving surface, and natural stream restoration providing the opportunity for interpretation and education. Vehicular access to this parking area can be accessed



Figure 21: Detail Plan - Centennial Park



Current Condition of Centennial Park

### LEGEND

- Existing Wetlands/ Fish Protection Area
  - Existing Trees
  - Streetscape Improvements/ New Tree Planting
  - Parking Area Improvements
  - Park/Open Space improvements
  - Beach Access Improvements
  - Floral Displays/Gardens
  - Walkway @2.0m wide
  - Proposed on-road Bicycling Route
  - Enhanced Pedestrian Route
  - Parking
1. Core Area Gateway
  2. Connection to Veterans' Ridge Park
  3. Look-out Platform/Rest Node
  4. Improved Ramp Access
  5. Playground Improvements:
    - w/Senior, Junior Accessible Play Elements
    - Armourstone Seating
  6. Picnic Shelter w/Group Seating/Tables
  7. Water Play Area
  8. Grass Amphitheatre w/Band Shell
  9. Waterfront Promenade w/Benches
  10. Naturalized Shoreline
  11. Beach Enhancements: cleanup
  12. Enlarged Beach area w/Armourstone Seating Edge
  13. Boat Launch Improvements
  14. Enhanced Naturalized/Native Stream Planting
  15. Event Parking, Pull-thru Trailer Space and Staging Area
  16. Swimming Platform Improvements

via Park Street; however, municipal services and pedestrian access to Centennial Park can also be provided via the Medical Centre property, while vehicular parking could be provided here during off-peak hours.

It is important to ensure that access is provided for all people to use the park. All elements within the Park shall be compliant with the Accessibility for Ontarians with Disabilities Act (AODA), including pathways and parking areas. A continuous walkway system that links to rest nodes, active areas, and facilities promotes accessibility for all. A small bandstand / gazebo and grass amphitheatre will provide the opportunity for outdoor performances. Use of permeable paving for the hardscape of walkways and plazas will help to minimize runoff into storm water systems.

Active recreation will be centralized in the current location and will utilize existing play structures, such as the ship, wherever possible. A new, separate Junior Play area will be wheelchair accessible and will include sensory play and swings. A central plaza and gazebo will provide a sheltered area for group seating and walkway connections to the beach, naturalized shoreline and waterfront promenade.

Figure 21 illustrates the detail plan for Centennial Park. Figures 22 and 23 (pages 54 and 55) show recommended specialty features, paving and banner details, and junior / senior and water play area details.

**LEGEND**

1. Streetscape Improvements
2. Road w/Shared On-road Bike Route
3. R.O.W varies
4. Road-side Parking Improvements
5. Walkway @2.0m wide
6. Stair/ Access Improvements
7. Improved Park Walkway/Connections
8. Play Area Improvements w/Junior Play Structure, Sensory Play and Swings
9. Park Plaza Area
  - a) Park Shelter w/Group Seating
10. Armourstone Slope Retention/ Seating Edge
11. Naturalized Shoreline Improvements
12. Waterfront Promenade along existing shoreline protection steel wall
13. Beach Improvements/Clean-up



Section Key Plan



SECTION G-G: Centennial Park with beach, gazebo, play area, and Main Street

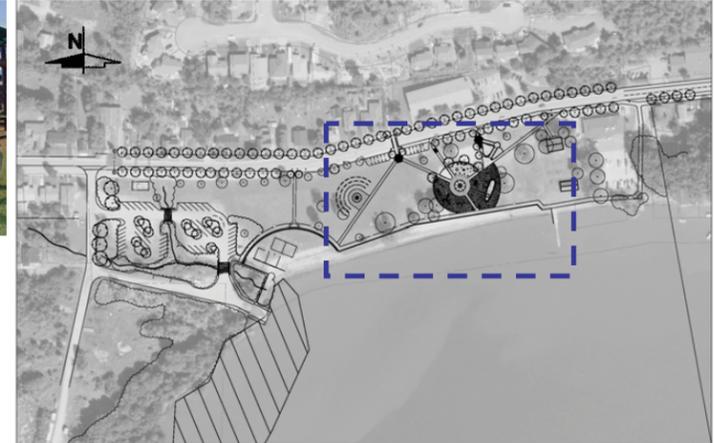
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## Special Features/Paving/Banners

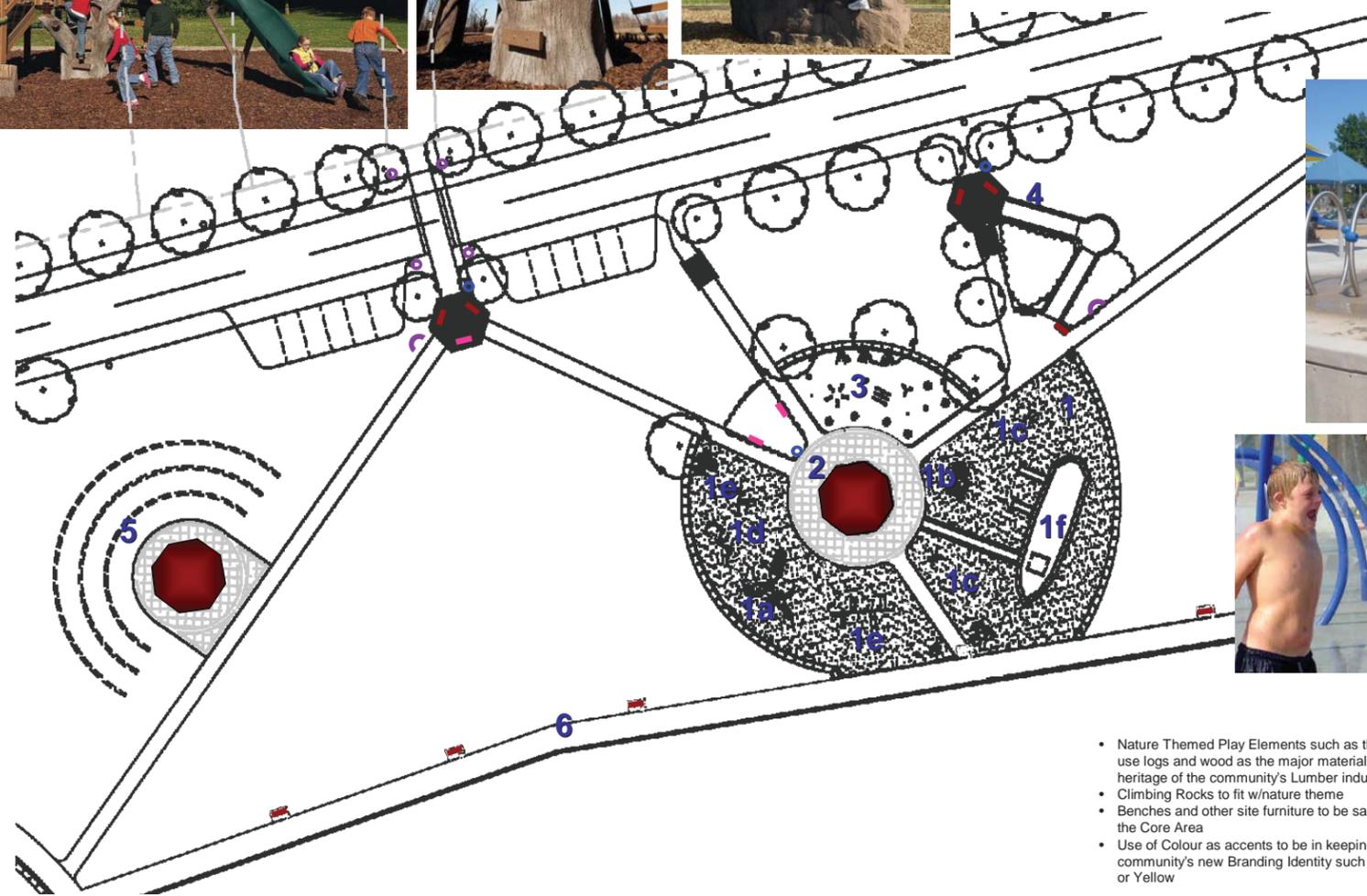


Figure 22: Centennial Park Special Features, Paving and Banner Details

# Junior and Senior Natural Play Area/Water Play



Key Plan



- LEGEND**
- WOOD/METAL GAZEBO
  - WOOD AND METAL BENCH W/BACK
  - WOOD AND METAL BACKLESS BENCH
  - TRASH/RECYCLE BINS
  - BICYCLE STORAGE
  - BANNERS ON WOOD POLES

1. Playground Improvements:
  - a) Natural Play Tree House
  - b) Rock Climbing w/Fossil Details
  - c) Log and Balance Beam
  - d) Sensory Play
  - e) TOT Swings w/Accessible Swing
  - f) Existing Play Ship
2. Picnic Shelter w/Group Seating/Tables
3. Water Play Area
4. Improved Access/Viewing Platforms
5. Grass Amphitheatre w/Band Shell
6. Waterfront Promenade w/Benches



- Nature Themed Play Elements such as the Tree house, use logs and wood as the major material to reflect the heritage of the community's Lumber industry.
- Climbing Rocks to fit w/nature theme
- Benches and other site furniture to be same throughout the Core Area
- Use of Colour as accents to be in keeping w/the community's new Branding Identity such as Red, Blue or Yellow

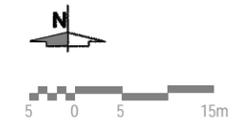


Figure 23: Centennial Park Junior and Senior Natural Play Area and Water Play Details

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Key Plan



Current Conditions of Waterfront Pier and Marina

## 4.6.2 Waterfront Place

Waterfront Place is a central feature within the Callander Downtown Waterfront Area. The intent is to create a major waterfront destination that will serve as a land/water gateway to the community for visitors from elsewhere on the lake as well as a destination for local residents and tourists. A number of features and facilities are proposed including rehabilitation of the wharf and boat launch, expanded marina, boardwalk along the water's edge, picnic shelter, washrooms, gazebo and wedding garden as well as a small parking lot.

Design of this people-oriented place on the waterfront would take its inspiration from the nautical and logging heritage themes expressed in a distinctive "compass" paving pattern at the turn-around, wood decking along the pier and nautical themed lighting fixtures, banners and architecture. Enhanced pedestrian walkways along Lansdowne Street will connect Waterfront Place to the main intersection in Downtown. Refer to Figure 24 for the Waterfront Place detail plan.

### Wharf Rehabilitation

The existing wharf requires substantial upgrades and rehabilitation to enable it to function as an effective breakwater, pedestrian promenade and outer edge to an expanded marina. The engineering review of the breakwater identified alternative strategies to rebuild the pier and provided an approximation of the financial implications from a capital and life cycle / operating perspective. However, to move forward, a detailed design is required to finalize the design and detailed costing in anticipation of construction. This should be initiated when design funding and capital funding is available. At this point in time, it is anticipated that funding may take some time to materialize, however, if funding conditions are favorable, the schedule for this initiative should be advanced.

### Marina Expansion

The plan also envisions development of an expanded marina within the basin. This opportunity could be developed to serve both local boaters as well as transient boaters from the lake. Local boaters would use the relocated boat launch ramp and tie-up slips on a day use or seasonal basis. Transient boaters would be attracted to Callander by the enhancements to the waterfront marina facilities and downtown public realm spaces contemplated in the Downtown Waterfront Revitalization Plan, as well as the convenience to existing and new downtown businesses such as restaurants and visitor-related services. It is recommended that an operational program be developed for the marina to determine the number and size of slips, level of servicing and related on-shore facilities required.

### Relocated Boat Launch

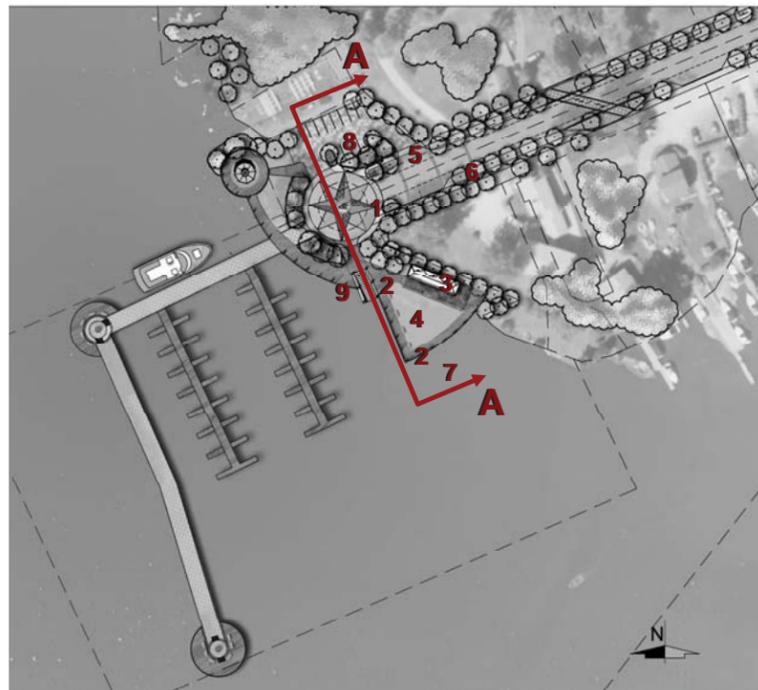
The boat launch is relocated to the inside of the protected marina basin. This will provide additional protected tie-ups and docking for day use boaters as well as calmer waters for launching and maneuvering. Trailer parking in relative proximity to the launch ramp is required. Two areas are identified for this purpose – one is the small parking area directly adjacent to the launch ramp for temporary parking immediately after launching and one at the north end of First Street where additional vehicles and trailers could be accommodated. Further, additional parking is identified in other areas in the Downtown.

### New Land-based Facilities

Several new land-based facilities are proposed in the plan. The shoreline boardwalk is an important integrating feature of Waterfront Place that links several components along the water's edge including the gazebo, wedding garden, picnic shelter, washroom building, trellis and picnic area as well as access to the municipal wharf.



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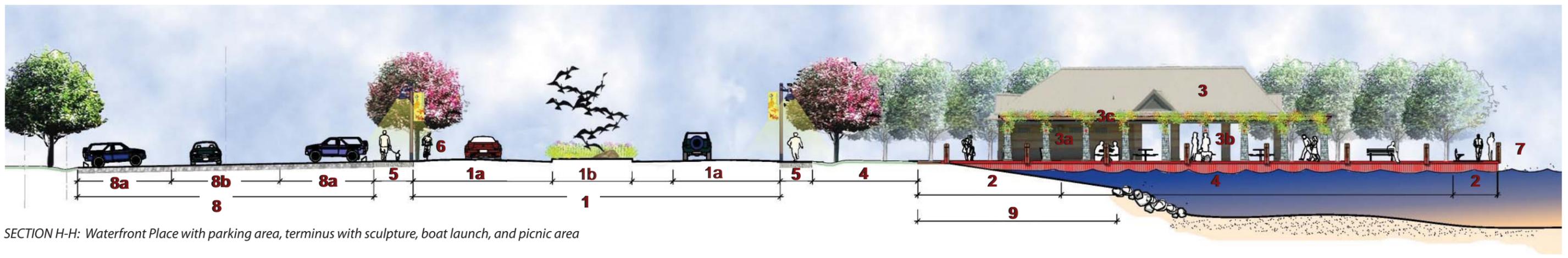
Section Key Plan

## LEGEND

1. Terminus @ Lansdowne Street
  - a) Driving Lane @ 9.0 m
  - b) Turning Circle w/ Sculpture
2. Boardwalk/Deck (Beyond)
3. Facilities Building:
  - a) Washrooms
  - b) Picnic Shelter w/ Group Seating/Tables
  - c) Trellis
4. Open Grass Picnic Area
5. Sidewalk @ 2.0m
6. Bike lane @ 1.75m
7. Temporary Mooring
8. Parking (13 Spaces + 2 on-street)
  - a) Parking Stall (@ 5.75m)
  - b) Driving Lane (@ 6.0m)
9. Boat Launch Ramp



Precedent Imagery



SECTION H-H: Waterfront Place with parking area, terminus with sculpture, boat launch, and picnic area

## 4.7 Built Form Design

The built form design guidelines address existing 1 and 2 storey buildings as well as recommendations that address future developments. These guidelines are intended to provide strong guidance and a framework for the development of a sense of place, identity and attractive character for the Callander Downtown Waterfront Area that reflects the shipping, lumbering and natural heritage design themes. These guidelines will describe the recommended built form for all buildings within the Downtown Waterfront Area, and in particular the commercial core within Callander, along Main Street and Lansdowne Street. This architectural emphasis will help to reinforce the core as the main service centre for the Municipality.

### 4.7.1 General Guidelines

- Encourage strong architectural tradition upon the original character that the core area was built;
- Encourage 2-3 storey commercial / mixed use building frontages;
- Encourage consistent design in facades and visual relatedness, to accomplish an integrated and cohesive Main Street;
- Encourage consistency in design and signage while recognizing the importance of creativity and diversity;
- Natural materials in signs with external lighting are preferred, and signs that enhance the architectural characteristics of the building are encouraged;
- Promote the removal of materials such as vinyl or aluminum siding and the restoration of original brick, block or wood facades. Where restoration is not possible, the use of siding of natural materials (i.e., real or replica wood) is encouraged to ensure the form and scale of the original façade is maintained.

### 4.7.2 Commercial Core Area

The Commercial Area of Callander (Main and Lansdowne Streets) will contain low to medium density mixed use (commercial - retail and services / residential) development. These buildings shall face Main Street and/ or Lansdowne Street as appropriate, providing the opportunity for commercial displays and sidewalk patios. Figure 25 illustrates recommended facade improvement guidelines for commercial buildings within the core area.

#### Guidelines:

- Architectural cues will be taken from local heritage including shipping, logging and lumbering heritage that has a historical presence in Callander. The building form and façade treatments within this mixed-use area will reflect this history;
- Traditional architecture and natural materials shall be incorporated into all designs to create an attractive, desirable and vibrant mixed-use area;
- Buildings may be free standing or built into a block design, and shall be complementary to the general character and scale of the area;
- Buildings located in visually prominent locations within the community, such as visible from the waterfront, Centennial Park and Highway 94, shall have a more detailed façade to highlight the unique character and high quality of the community.

### 4.7.3 Residential Areas

Located throughout the Downtown Waterfront Area, these areas shall have low to medium density residential development.

#### Guidelines:

- Traditional architecture and natural materials shall be incorporated into all designs to create consistency among buildings within the core area;
- Building form and facade treatments shall highlight the local heritage;
- Residential buildings located in visually prominent locations shall have a more detailed facade to highlight local character.



Built Form Design Inspiration - Historic Buildings in Callander

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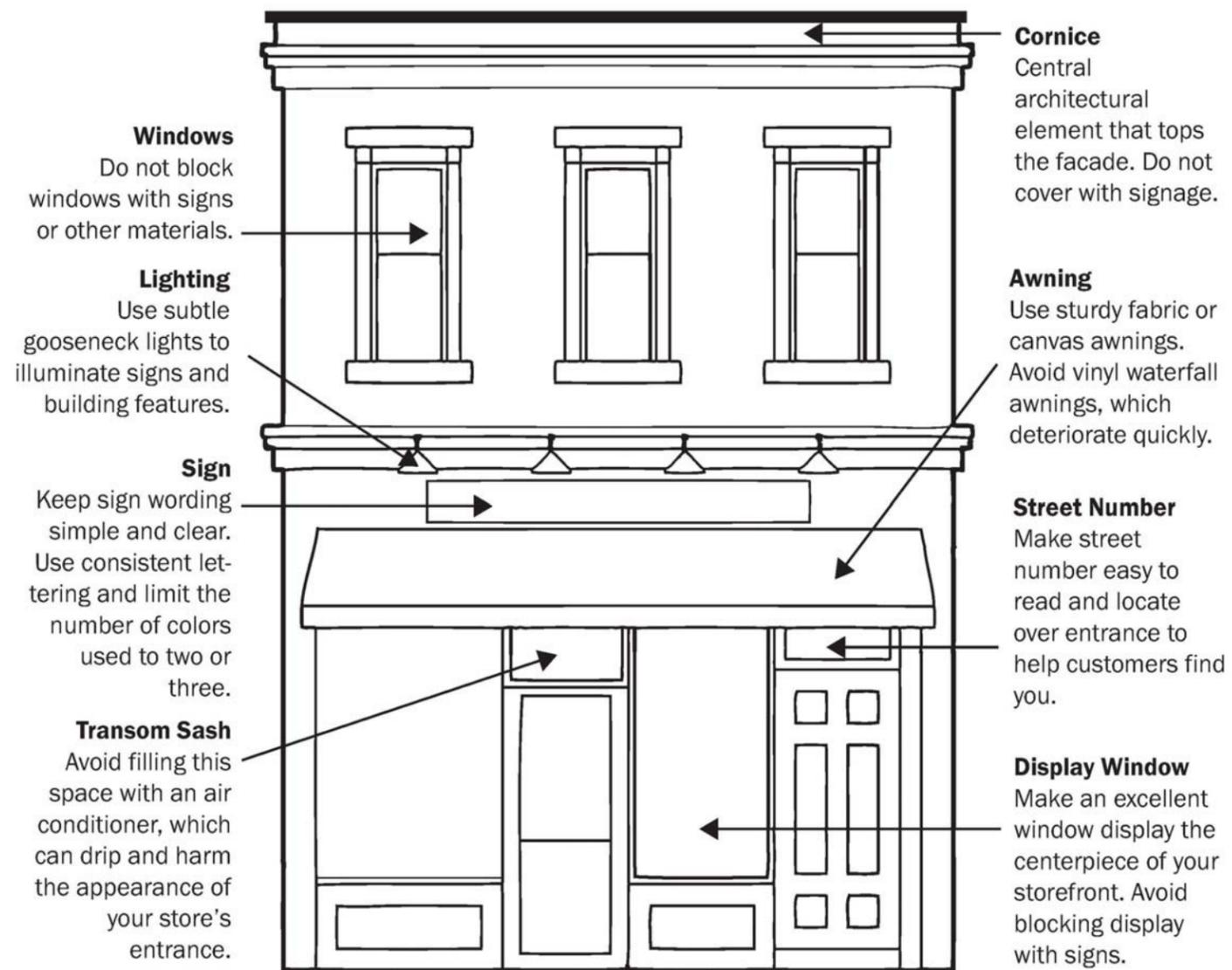


Figure 25: Typical Facade Improvement Guidelines



Precedent Imagery

5.0

implementation strategy





## 5.1 Action Plan

The achievement of the Downtown Waterfront Revitalization Master Design Strategy will require public sector involvement of the Municipality of Callander, private sector involvement in terms of development plans and requirements of private land owners and public / private partnerships where the Municipality and landowner will need to work jointly to achieve common goals. A variety of planning and financial tools will be required to implement the strategy. The Community Improvement Plan (CIP), developed in conjunction with the strategy, is designed to promote redevelopment and physical improvements of the Downtown Waterfront Area, which might prove to be a catalyst for broader positive social and economic impacts. The CIP will also help focus private investment in improving the aesthetics of the downtown waterfront core.

In order to achieve the principles identified within the Master Design Strategy, an action plan has been developed. Each principle can be realized through a number of specific projects. This action plan enables the Municipality to achieve the strategy's overall vision through the implementation of projects in short-, mid- and long-term time frames. Each of the eight principles, as identified in Section 3.0 Vision and Design Principles, have action projects associated with them that will allow the Municipality to incrementally accomplish the implementation of the Master Design Strategy.

It should be noted that prior to commencing any phases of the Action Plan and Implementation Strategy, sewage treatment capacity and parking capacity in the core area need to be considered. While they are not directly related to the Strategy, these issues may potentially affect the components of the Master Design Strategy and / or the sequence in which they are implemented.

### ***Make Callander a "Destination of Choice"***

Enhancement to the existing core area, particularly at the main intersection, in order to create a vibrant, safe and picturesque community, is an important action to achieve this principle. The core area must be pedestrian-oriented and accessible to all to create a sense of belonging, as well as provide areas for community gathering and social interaction. Expanding the range of uses within the Downtown Waterfront Area and improving the streetscape, including sidewalks, lighting, street furniture, etc., will attract visitors, boosting the local economy and enriching the livelihood of both permanent and seasonal residents.

#### ***Action Projects***

- Streetscape Improvements at Main Intersection (Signature Parkette)
- Lansdowne Street Streetscape Works (Waterfront to Highway 94)
- Main Street Streetscape Works at Centennial Park

### ***Promote Cultural Excellence***

This principle can be realized through encouraging and promoting cultural events and opportunities to locate or take place within the Callander Downtown Waterfront Area. Adequate indoor / outdoor gathering and events space for festivals, markets, theatre performances, concerts, etc. is imperative for cultural excellence. Key heritage elements within Callander shall be embraced and celebrated and will facilitate civic pride of these unique features.

#### ***Action Projects***

- Amphitheatre / Outdoor Performance Space at Centennial Park

*The Action Plan identifies key projects that will realize the overall vision and principles for the Master Design Strategy.*

*Priority levels for each key project are identified in the Implementation Summary Matrix.*



# implementation strategy

## **Further Develop Callander as a Gateway and Hub**

Callander's Downtown Waterfront Area has the potential to become a key gateway to / from Callander Bay and Lake Nipissing. A cohesive and comprehensive system of gateways and signage will enable visitors to become easily oriented and have a pronounced sense of place. The improvement of the pier and the development / expansion of the marina facilities will attract more visitors to Callander's waterfront, while providing adequate community services, facilities and amenities to ensure that Callander serves as a hub, or service centre, for residents, cottagers, tourists and visitors of all types during all seasons. This is imperative to create a distinct and attractive community on Lake Nipissing.

### **Action Projects**

- Core Area Gateways
- Minor Gateway Signage
- Downtown / Identity Signage
- Interpretive Signage
- Waterfront Gateway (streetscape works)
- Pier Improvements and Marina Development

## **Encourage Year Round Uses**

Year round recreational activities, leisure opportunities, and cultural and entertainment events within Callander shall be actively promoted to enhance the quality of life for residents, tourists and visitors alike. Winter tourism opportunities should also be promoted in order to sustain the tourism industry in all seasons. A number of opportunities exist related to ice fishing, snowmobiling, cross country skiing, snowshoeing, ice skating, etc.

### **Action Projects**

- Ice Hut Staging Area Improvements (Centennial Park Parking Area)
- Trail Connections (snowmobiling, cross country skiing, etc.)
- Designated Ice Skating Area at Pier

## **Develop Callander as a Green and Sustainable Community**

Callander should actively promote itself as a sustainable community - environmentally, economically and socially. Through the protection of the surrounding natural environment, re-naturalization of streams and other natural features, encouraging the use of active modes of transportation (bicycle, pedestrian, etc.), improving the pedestrian realm, encouraging sustainable design practices in all new development (building and site design), and providing interpretive signage for sustainable elements of new development to increase "ecological literacy" within the community, Callander can pride itself on the unique environmental surroundings that are an integral part of the community.

### **Action Projects**

- Parking Area and Shoreline Naturalization at Centennial Park
- Non-motorized Boat Launch Access Improvements
- Bicycle / Pedestrian Linkages throughout Core Area
- Storm Water Outlets / Treatments
- Dark Sky Lighting
- Interpretive / Educational Signage

## **Enhance Marina Facilities and Services**

Enhancing the marina facilities and services within the Callander Downtown Waterfront Area is extremely important to the overall vitality and economic development of the Downtown Waterfront Area. Improvements to the Callander wharf (pier) is necessary to be undertaken in order for Callander to be able to promote itself as a destination on Callander Bay and beyond to Lake Nipissing. Providing adequate facilities and services for boaters and cottagers is also an important aspect of the overall sustainability of the community.

### **Action Projects**

- Pier Improvements and Shoreline Protection
- Marina Docking (Municipality to partner with private operator)
- Boat Launch Improvements (to be used for motorized boat launching)

### ***Provide Multi-Use Recreational Facilities***

Developing and / or improving a variety of multi-use recreational facilities within the Callander Downtown Waterfront Area that create the opportunity for active transportation and recreation is important. These facilities include a well-linked multi-use trail system in the community with connections to the Kate Pace Way and regional trail system, as well as surrounding communities; an improved Centennial Park with expanded facilities; and, enhanced waterfront area at the foot of Lansdowne Street with improved facilities and public spaces.

#### ***Action Projects***

- Centennial Park Improvements
  - Water Play Area (Splash Pad)
  - Beach Improvements and Naturalization
  - Play Area Improvements
  - Beach Volleyball Area
- Waterfront Promenade Development
  - Urban Beach
  - Concession Building / Picnic Shelter
  - Waterfront Boardwalk

### ***Identify and Promote Economic Development Opportunities***

The development of economic opportunities and mixed-use development within the Downtown Waterfront Area is a key component for realizing the overall vision for Callander. Promoting tourism development, such as accommodations, services and amenities will provide employment and increase the number of visitors to Callander. Professionals (i.e., Engineers, Architects, Medical professionals, etc.) are to be encouraged to locate offices within the Downtown Waterfront Area to further develop the economic base.

A diversified housing strategy that addresses young families and seniors needs is also important for the sustainability of the community. This population will help to sustain businesses throughout the year. Providing support to appropriate private sector development is also important to advance the economy within Callander.

#### ***Action Projects***

- Marina Development (i.e., docks, services areas, etc.)
- Potential Downtown Redevelopment Sites / In-filling (see Section 4.2.7 for details)
- Private Sector Property Improvements (e.g., Facade treatments, parking lot upgrade / expansion, etc.)

# implementation strategy

## 5.2 Phasing Plan

The development process required to undertake each of these projects involves a detailed design, tendering and construction process including:

- Design development;
- Working drawings and technical specifications;
- Preparation of contract documents;
- Tendering;
- Selection of Contractor;
- Initiation of construction; and,
- Contract administration.

Implementation of the Action Projects for the Master Design Strategy will require a number of phases over several years. The phasing plan is intended to begin with some initial capital expenditures to establish a basic site infrastructure and proceed over time to a more extensive level of development and eventual full build-out.

The phasing plan is summarized below and the Action Projects have been categorized in each time frame in the accompanying Table 6: Implementation Summary Matrix (see page 69).

**Table 2: Phasing Plan Summary**

Priority Level	Project Term	Time Frame	Description
HIGH	Short-Term	1-5 years	Action projects identified to be implemented in the short-term are considered “Quick Wins”, projects that can be completed fairly easily for a reasonable cost.
MEDIUM	Mid-Term	5-10 years	Projects identified to occur in the mid-term are not as pertinent and more costly; however, they contribute considerably to achieving the overall vision for the Downtown Waterfront Area. It should be noted that the Pier Improvement is identified as a mid-term project, this is due to the large cost associated with the project. Once the appropriate funding is in place, this project should become a priority.
LOW	Long-Term	10-15+ years	Action projects identified for long-term implementation are improvement type projects and are not immediately imperative for the successful implementation of the Master Design Strategy.

## 5.3 Potential Economic Benefits

The program outlined in the Downtown Waterfront Revitalization Master Design Strategy includes a wide range of programs aimed at improving and maximizing the use and attractiveness of the downtown (including streetscape enhancements and amenities for the core area, potential redevelopment sites, identification of additional parking and service opportunities, pedestrian system linkages with existing trail systems, wayfinding systems, among others) and the Municipality’s key waterfront assets (including public parks, the municipal dock and breakwater, expansion of marina services and facilities, as well as future opportunities for other tourist related businesses, including tour boat services and operations, among others). These initiatives and opportunities will contribute to strengthening the local economy with a more diversified business mix and additional employment opportunities, increased customer traffic for existing businesses and identification of gaps in current services, tourist attractions and development opportunities (both public and private).

The implementation / action plan contained in this Strategy identifies a program of actions that are based on priorities that were identified through the community engagement program and discussions with the Steering Committee and Council. These priorities are organized as high priority projects (quick wins), medium priority projects (requiring significant additional funding), and low priority projects that can be coordinated with the longer term investments in major infrastructure envisioned by the Municipality.

Potential economic benefits of these programs will result in a range of construction and permanent employment opportunities (direct and indirect) in both the public and private sectors. Construction related employment would result from the investments in community improvements identified in the program. Normally, approximately 50% of the construction costs are associated with materials purchases (this also includes some labour), and 50% of the construction costs are associated with labour costs.

**Table 3: Construction Value Break-down by Priority Level**

	Construction Value	Materials (50%)	Labour (50%)
<b>Project Priority</b>			
High Priority (0-5 years)	\$797,500	\$398,750	\$398,750
Medium Priority (5-10 years)	\$8,014,000	\$4,007,000	\$4,007,000
Low Priority (10-15+ years)	\$3,375,000	\$1,687,500	\$1,687,500

Permanent employment opportunities in both the public and private sectors would also result from the program as proposed. Public sector jobs would be required within the Municipality, including staff to handle the additional maintenance and operational requirements resulting from the additional investments in streetscapes, park facilities, trails, waterfront and marina facilities. Additional costs would include tree pruning, planter watering and weeding, additional sidewalk and pedestrian area snow plowing (not including roads) and maintenance would require annual allowances of a minimum of approximately \$10,000 / km / year (mainly labour) for streetscapes and related works, and approximately \$2.50 / sq. m / year for intensively used park and waterfront areas. Annual operating budgets would require review and augmentation to address these functions that are well beyond those that currently exist in the Municipality. The labour involved in addressing these tasks would be required as permanent public sector jobs.

Additional permanent jobs would be associated with the new and expanded private sector business opportunities. These would include business opportunities at:

- Expanded marina at the public dock area potentially including maintenance, operational and administrative jobs associated with the boat launch, boat slips and marina office;
- Tour boat operation based at the dock including the on-shore and on-board jobs during the tourist / boating season and off season maintenance and administrative jobs;
- Concession and washrooms associated with the marina and dock area including maintenance, administrative and serving jobs; and,
- Tourist / sports equipment outfitter and guide services (may include rentals of kayak, canoe and fishing boats, fishing tackle and bait store, guiding operations, camping equipment, cross-country skiing, snowshoeing and mountain bike equipment rentals, etc.), located in the downtown or at the marina area.

Another source of permanent private sector jobs would be provided through the provision of additional office space and facility development to accommodate the identified gaps in business and commercial/professional services in the community (identified in the Phase 1 Report) including additional medical and dental services, tourist and hospitality / overnight accommodation businesses, other professional services that may include engineering, architect, accountant, lawyer and others. These would provide a combination of professional, administrative and service jobs in the community and these workers would potentially require residential accommodation within the Municipality.

All of these development programs, including the public sector investments in infrastructure, facilities and amenities, together with the private sector development opportunities and the resulting demand for additional residential development will create a strong requirement for all municipal services including sewage treatment facilities. The sewage treatment facility is at or near capacity and a major investment to upgrade this crucial facility is required to support the revitalization program as described in this Strategy.

**5.3.1 Baseline Municipal Employment Projections**

The Municipality of Callander Growth Management Study of 2008 included a projection of existing or future jobs in the Municipality of Callander. It also included a projection of labour force growth in the Municipality over the period of 2006-2026. Specifically, as of 2006, based on a job ratio of 1 job per 4 residents (jobs located in the Municipality), there are some 895 jobs as of 2006 rising to 1,145 jobs in 2026 based on forecast residential growth. These figures are used to provide input to the 2009 Development Charges (DC) Background Study. The NET anticipated employment gain is 250 jobs over the period from 2006-2026.

These jobs are what can be generally referred to as population-related jobs; those are jobs that are created as a result of population growth. These estimates do not account for the possibility of other employment investment occurring in the Municipality which is otherwise unrelated to population growth. Due diligence of these estimates has been undertaken, with specific regard to the ratio of population-related employment growth to population growth. The ratio of 0.25 is similar to other population-related employment ratios expressed as Activity Rates<sup>1</sup>. For example, in North Bay, the 2009 DC Background Study projects growth in the ratio from around 0.211 to 0.234. This activity rate is similar to that identified in the Callander Growth Management Study of 2008.

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### 5.3.2 CIP Investment-Related Employment Impacts

More relevant to the Downtown Waterfront Revitalization Strategy is the potential associated with the Community Improvement Plan (CIP) to generate employment growth. This can be done as a theoretical exercise with respect to the likely construction-related employment growth. The assumptions of an estimate of employment impact (direct) are as follows:

- \$50,000 per year expended through the Community Improvement Plan, summing to \$250,000 over 5 years;
- Based on evidence from other municipalities, a minimum private sector investment of \$5 to \$6 per dollar expended by the public sector is necessary. On this basis, a reasonable assumption is that the CIP may generate a minimum \$1.5 million of private sector construction activity.
- Based on this, projections of direct employment gain and indirect employment gain can be assessed and the results provided as follows:

**Table 4: CIP-Related Construction Employment Impacts based on \$1.5 million public/private investment**

	Direct (Person Years FTE <sup>2</sup> )	Indirect and Induced (Person Years FTE <sup>3</sup> )	Total Employment (Person Years FTE)
Construction	4	2	6
Consulting (other)	2	1	3
<b>Total</b>	<b>6</b>	<b>3</b>	<b>9</b>

Source: Sierra Planning & Management Economic Impact Model, utilizing Statistics Canada Industry Accounts Division, System of National Accounts, Input-Output Tables, Province of Ontario

As noted, these are construction-related employment estimates. Should the CIP provide renewed investment in Downtown Callander, this will contribute to the on-going sustainability of the existing commercial businesses in the community, resulting in the retention or expansion of permanent job opportunities in Callander.

It is recommended that with the implementation of the Community Improvement Plan (CIP), monitoring of the amount of private sector expenditure on property improvement should be undertaken to create a baseline of information on the leverage of private sector expenditure for every dollar of public sector

investment through the programs which comprise the CIP. Moreover, the number of jobs in those commercial properties under improvement should also be monitored as a baseline for potential job retention / expansion benefits arising (at least in part) from the programs of the CIP.

### 5.3.3 Construction-Related Employment Impacts

The employment generated from the construction of the various public realm improvements which comprise the Strategy over more than 15 years represents a one-time impact arising from the capital expenditures on design and construction. The following outlines the order of magnitude employment impacts associated with the projects identified in the Master Design Strategy.

**Table 5: Construction-related Employment Impacts based on \$12.2 million construction value**

	Direct (Person Years FTE <sup>2</sup> )	Indirect and Induced (Person Years FTE <sup>3</sup> )	Total Employment (Person Years FTE)
Construction	60	30	90
Consulting (other)	13	7	21
<b>Total</b>	<b>73</b>	<b>37</b>	<b>111</b>

Source: Sierra Planning & Management Economic Impact Model, utilizing Statistics Canada Industry Accounts Division, System of National Accounts, Input-Output Tables, Province of Ontario

It should be noted that these estimates comprise the combined total of employment generated over the period of implementation. If the capital expenditures are subject to change, so too are the estimates of employment. In reality, these jobs are created over time and do not necessarily represent net new jobs to the region. Impacts of wage inflation, availability of trades and other time and location specific factors will influence the nature and extent of employment associated with the public works and whether this employment is local or sourced from outside of the local area.

<sup>1</sup> Activity Rate is the ratio of employment (jobs) to population in a municipality

<sup>2</sup> Person-years of employment is defined as a full-time equivalent (FTE) employment position for 1 year.

<sup>3</sup> Direct impacts comprise of employment directly related to the project; indirect employment impacts comprise of employment impacts in those industries which supply the construction project; induced impacts include the impacts throughout the wider economy as a result of the discretionary spending of employment income generated by the direct and indirect impacts.

### 5.4 Implementation Summary Matrix

The following summary matrix provides proposed projects identified within the master design strategy for implementation.

**Table 6: Implementation Summary**

DESIGN PRINCIPLE	PROJECT	PRIORITY LEVEL	CAPITAL COSTS	ISSUES + IMPACTS	POTENTIAL FUNDING SOURCES	MANAGEMENT REQUIREMENTS
<i>Make Callander a "Destination of Choice"</i>	Streetscape Improvements at Main Intersection (Signature Parkette)	HIGH	\$375,000	<ul style="list-style-type: none"> <li>Burying of overhead wires at main intersection (at a minimum) is recommended. This is to be considered in light of the costs to do these improvements.</li> <li>During streetscape improvements, conduit may be installed (at a small additional cost) to accommodate underground wires in the future.</li> <li>Ensure intersection design accommodates truck turning movements (as Lansdowne Street is identified as preferred truck route).</li> </ul>	<ul style="list-style-type: none"> <li>Hydro One may contribute funds identified for upgrading of existing facilities / poles to go towards funding the burying of wires (e.g. Ontario Street in Stratford based on a cost-sharing formula with Festival Hydro).</li> <li>Funding from senior levels of government is required to enrich this project to achieve economic development and community enhancement objectives.</li> </ul>	<ul style="list-style-type: none"> <li>Approximately 6 parking spaces are lost with this project.</li> <li>Additional convenient parking is required in close proximity to compensate for this loss.</li> <li>Maintenance will be required to support the amenities in this area including tree and flower planting, irrigation, spring and fall clean-up and litter removal.</li> </ul>
	Lansdowne Street Streetscape Works Phase 1: Main Street to Highway 94 Phase 2: Main Street to Waterfront	MEDIUM	\$1,927,000	<ul style="list-style-type: none"> <li>Phase 1 is in the preliminary planning stages at the time this report is prepared. Additional funding is required to accomplish the streetscape improvements beyond the road re-construction costs.</li> <li>Phase 2 is a future project, but also requires streetscape improvements in addition to road re-construction.</li> <li>It should be noted that there is overlap between the streetscape costing and the road reconstruction costing (prepared by others) for such items as sidewalk, lighting, etc.</li> </ul>	<ul style="list-style-type: none"> <li>Funding from senior levels of government is required to enrich these projects to achieve economic development and community enhancement objectives.</li> </ul>	<ul style="list-style-type: none"> <li>During the detailed design phase, coordinate streetscape improvements with road re-construction design work to ensure that they are fully integrated relative to street light, signage and street tree locations, pedestrian crosswalks, cycle lanes and on-street parking.</li> <li>During the construction phase, coordinate with municipal officials, merchants and general public on a DAILY basis to ensure that access and inconveniences are minimized.</li> </ul>
	Main Street Streetscape Works (Park Street to Rock Street, includes streetscape works at Centennial Park)	LOW	\$3,150,000	<ul style="list-style-type: none"> <li>Enhances the arrival experience through the community.</li> </ul>	<ul style="list-style-type: none"> <li>Coordinate streetscape enhancements with future road reconstruction where possible.</li> <li>Funding from senior levels of government is required to achieve economic development and community enhancement objectives.</li> </ul>	<ul style="list-style-type: none"> <li>During the detailed design phase, coordinate streetscape improvements with road re-construction design work to ensure that they are fully integrated relative to street light, signage and street tree locations, pedestrian crosswalks, cycle lanes and on-street parking.</li> <li>During the construction phase, coordinate with municipal officials, merchants and general public on a DAILY basis to ensure that access and inconveniences are minimized.</li> </ul>
	Waterfront Park Improvements	MEDIUM	\$1,580,000	<ul style="list-style-type: none"> <li>Provides a new destination and people place on the waterfront.</li> <li>Presents economic development, recreation and tourism opportunities.</li> <li>Works shall be coordinated with Pier and Marina Improvements.</li> </ul>	<ul style="list-style-type: none"> <li>Funding from senior levels of government is required to enrich these projects to achieve economic development and community enhancement objectives.</li> </ul>	<ul style="list-style-type: none"> <li>On-going maintenance and operations required.</li> <li>Additional municipal infrastructure and servicing requirements (i.e. water, sewer, electrical).</li> </ul>

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DESIGN PRINCIPLE	PROJECT	PRIORITY LEVEL	CAPITAL COSTS	ISSUES + IMPACTS	POTENTIAL FUNDING SOURCES	MANAGEMENT REQUIREMENTS
Promote Cultural Excellence	Amphitheatre / Outdoor Performance Space (Includes shelter, site grading, lighting and electrical outlets for a variety of users)	MEDIUM	\$50,000 - \$100,000	<ul style="list-style-type: none"> <li>Enhances cultural diversity and quality of life for residents and visitors providing an outdoor venue for events and celebrations.</li> <li>Integrate park design and site grading.</li> </ul>	<ul style="list-style-type: none"> <li>This is an ideal project for a community based fundraising effort that may also include service club involvement and /or sponsorships.</li> </ul>	<ul style="list-style-type: none"> <li>On-going programming and seasonal usage is important to keep the venue active and alive.</li> <li>Design must be as vandal proof as possible.</li> <li>Maintenance is required.</li> </ul>
Further Develop Callander as a Gateway and Hub	Community Gateway	HIGH	\$18,750	<ul style="list-style-type: none"> <li>Through unique design rooted in the nautical and logging heritage, this important gateway will enhance the identity and character of the community.</li> </ul>	<ul style="list-style-type: none"> <li>The overall gateways, signage and wayfinding project will require additional funding for senior levels of government to achieve economic development and community enhancement objectives.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required.</li> </ul>
	Minor Gateway Signage	HIGH	\$37,500	<ul style="list-style-type: none"> <li>Smaller scale gateway signage identifying the core area.</li> </ul>	<ul style="list-style-type: none"> <li>Same as above.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required.</li> </ul>
	Directional / Identity Signage	HIGH	\$25,000	<ul style="list-style-type: none"> <li>Provides wayfinding for visitors and residents as part of the overall signage package.</li> </ul>	<ul style="list-style-type: none"> <li>Same as above.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required.</li> </ul>
	Interpretive Signage	HIGH	\$18,750	<ul style="list-style-type: none"> <li>Education and Interpretation of selected natural and cultural heritage features.</li> </ul>	<ul style="list-style-type: none"> <li>Same as above.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required.</li> </ul>
	Waterfront Gateway (streetscape)	MEDIUM	\$220,000	<ul style="list-style-type: none"> <li>A significant feature to enhance arrival and significance of the waterfront for those arriving and departing.</li> </ul>	<ul style="list-style-type: none"> <li>Same as above.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required.</li> </ul>
	Pier Improvements and Marina Development	MEDIUM	See costing provided below	<ul style="list-style-type: none"> <li>This project recognizes that Callander's waterfront functions as a gateway to the downtown, and as such requires major improvements to support it as a marine centre and to achieve economic development objectives.</li> <li>While this project is identified as Medium Priority, due to the significant funding requirements it is important to initiate detailed feasibility and technical design work on the project in the short term.</li> </ul>	<ul style="list-style-type: none"> <li>Refer to funding comments below.</li> </ul>	<ul style="list-style-type: none"> <li>Further study should be undertaken to determine the necessary wharf length to provide adequate protection for marina operations.</li> <li>Ongoing maintenance required.</li> </ul>

DESIGN PRINCIPLE	PROJECT	PRIORITY LEVEL	CAPITAL COSTS	ISSUES + IMPACTS	POTENTIAL FUNDING SOURCES	MANAGEMENT REQUIREMENTS
<i>Encourage Year Round Uses</i>	Ice Hut Staging Area (Centennial Park Parking Area)	MEDIUM	Included in Parking Area Naturalization	<ul style="list-style-type: none"> <li>The enhancements to this area provide continued lake access during winter months for ice fishing.</li> </ul>	<ul style="list-style-type: none"> <li>Funding assistance for parking lot improvements and naturalization are required.</li> <li>May be available as part of source water protection and / or naturalization funding (e.g., Conservation Authority).</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required – particularly annual spring clean-up.</li> </ul>
	Trail Connections (pedestrian, cycle, snowmobile, ski)	HIGH	Included in other projects	<ul style="list-style-type: none"> <li>Enhanced trail connections will support tourism and local recreational needs.</li> </ul>	<ul style="list-style-type: none"> <li>Funding assistance for year-round trail development will be required.</li> <li>Sources may include Ministry of Health Promotion (MHP - Healthy Communities Fund), FedNOR, NOHFC, Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Ongoing maintenance required – potential to enlist support of volunteer / stewardship groups and / or cross country skiing, snowmobile, hiking, cycling clubs.</li> </ul>
	Ice Skating Area at Pier	MEDIUM	Included in Pier Improvement	<ul style="list-style-type: none"> <li>Protected area in the shelter of the pier for winter recreational skating on the waterfront as an alternative to the roofed skating rink at the Bill Barber Complex.</li> </ul>	<ul style="list-style-type: none"> <li>Costs primarily related to maintenance and operations.</li> </ul>	<ul style="list-style-type: none"> <li>A seasonal project that could be undertaken by Municipal staff and / or volunteers and supported by the Municipality.</li> </ul>
<i>Develop Callander as a Green and Sustainable Community</i>	Centennial Park Parking Area Naturalization	MEDIUM	\$175,000	<ul style="list-style-type: none"> <li>Naturalization will create a more attractive parking area in this location while enhancing water quality, habitat diversity and visual character of the waterfront.</li> </ul>	<ul style="list-style-type: none"> <li>Funding assistance for parking lot improvements and naturalization are required.</li> <li>Sources may include Conservation Ontario (shoreline naturalization, habitat creation and planting), FedNOR, NOHFC and Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Coordinate annual spring clean-up by Municipal staff and / or volunteer stewardship groups.</li> </ul>
	Non-Motorized / Shallow-Draft Boat Launch Access Improvements	HIGH	\$37,500	<ul style="list-style-type: none"> <li>Promotes the green / sustainable community goal and provides alternative recreational boating access to Callander Bay.</li> </ul>	<ul style="list-style-type: none"> <li>Funding assistance for parking lot improvements and naturalization are required.</li> <li>Sources may include Conservation Ontario (shoreline naturalization, habitat creation and planting), FedNOR, NOHFC and Trillium</li> </ul>	<ul style="list-style-type: none"> <li>Water depth is shallow in this location and does not support use by larger, deep draft boats – this restriction will require appropriate signage and monitoring.</li> <li>Small floating dock (with anchoring system) may be required at this location.</li> </ul>
	Bicycle / Pedestrian Linkages Throughout Core Area	MEDIUM	Included in Streetscape Works	<ul style="list-style-type: none"> <li>Contributes to a more healthy and accessible community, promotes alternative / active transportation and the concept of “Complete Streets”.</li> </ul>	<ul style="list-style-type: none"> <li>Funding assistance for year-round trail development will be required.</li> <li>Sources may include Ministry of Health Promotion (MHP - Healthy Communities Fund), FedNOR, NOHFC, Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Bike parking loops are required in parks, on streets and other public spaces.</li> <li>Use within the on-street sections may be compromised by winter snow plowing / storage requirements.</li> <li>Spring clean-up is required to remove salt, sand and debris.</li> </ul>
	Storm Water Outlets / Treatment	HIGH	Refer to Costing Provided by Others	<ul style="list-style-type: none"> <li>Re-location of outlets and state-of-the-art water quality treatment of stormwater supports the green / sustainable community goals and contributes to source water protection program.</li> </ul>	<ul style="list-style-type: none"> <li>Refer to costing provided by others.</li> </ul>	<ul style="list-style-type: none"> <li>Coordinate with other infrastructure improvements such as streetscapes, road rebuilding and other projects.</li> </ul>
	Dark Sky Lighting	MEDIUM	Included in Streetscape and Park Works	<ul style="list-style-type: none"> <li>Promotes the green / sustainable community goal and supports economic development objectives by contributing to overall quality of community space through public sector investment.</li> </ul>	<ul style="list-style-type: none"> <li>Funding from senior levels of government is required to carry out this major project to achieve economic development and community enhancement objectives.</li> <li>Funding sources may include FEDNOR / NOHFC / Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Technical improvements to LED lighting are making these types of light sources less glaring, more efficient and more affordable for street lighting purposes.</li> </ul>

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DESIGN PRINCIPLE	PROJECT	PRIORITY LEVEL	CAPITAL COSTS	ISSUES + IMPACTS	POTENTIAL FUNDING SOURCES	MANAGEMENT REQUIREMENTS
<b>Enhance Marina Facilities and Services</b>	Pier Improvement + Shoreline Protection	MEDIUM	\$2,500,000	<ul style="list-style-type: none"> <li>The existing pier can be rehabilitated according to the recommendations of the Baird Report (2011), see Appendix 2 for details.</li> <li>These costs may be reduced if the length of the pier / breakwater can be shortened – but is dependent on the size of marina anticipated.</li> </ul>	<ul style="list-style-type: none"> <li>Funding from senior levels of government is required to carry out this major project to achieve economic development and community enhancement objectives.</li> <li>Funding sources may include FEDNOR / NOHFC / Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Enhancements to pier and surrounding area may require hiring additional municipal staff (i.e., maintenance, concession, rentals / outfitters, etc.).</li> </ul>
	Marina Docking + Improvements (also includes rest nodes, shade structures and benches)	MEDIUM	\$915,000	<ul style="list-style-type: none"> <li>Precise number of boat slips to be determined based on market analysis by private operator.</li> <li>Marina area capacity may accommodate twice the former number of slips if warranted.</li> </ul>	<ul style="list-style-type: none"> <li>Private Partnership (for marina development and operations).</li> </ul>	<ul style="list-style-type: none"> <li>Marina may be privately operated -a potential new private sector business opportunity.</li> </ul>
	Boat Launch Improvements	MEDIUM	\$100,000	<ul style="list-style-type: none"> <li>Parking for vehicles and trailers will be required within the downtown waterfront study area. (e.g., at north end of First Street).</li> <li>Ensure required vehicle turning movements are provided as part of design.</li> </ul>	<ul style="list-style-type: none"> <li>Coordinate this project with the construction of the pier and shoreline improvements as one project.</li> <li>Senior government funding is required from FEDNOR / NOHFC / Trillium.</li> </ul>	<ul style="list-style-type: none"> <li>Additional operational and maintenance required.</li> <li>Ramp may also serve as alternative vehicular access to ice during winter months.</li> </ul>
<b>Provide Multi-Use Recreational Facilities</b>	Beach Improvements and Naturalization at Centennial Park	MEDIUM	\$57,000	<ul style="list-style-type: none"> <li>Requires partial sheet pile removal, re-grading, sand replenishment on expanded beach surface and replanting of native species in naturalized areas.</li> <li>Bark accumulation on beach is ongoing, and can only be cleaned up as necessary. Any long-term solutions for this are not considered ecologically feasible at present.</li> </ul>	<ul style="list-style-type: none"> <li>Cost of sheet pile removal, re-grading and sand replenishment may be accomplished by municipal forces.</li> <li>Planting may be a community involvement project.</li> <li>Consider bark clean-up as an annual maintenance and operational task.</li> </ul>	<ul style="list-style-type: none"> <li>Sand may be brought in annually from a local source to replenish and revitalize the beach area for summer use.</li> </ul>
	Play Area Improvements at Centennial Park	LOW	\$225,000	<ul style="list-style-type: none"> <li>Since the existing play structure is quite popular, enhancements to the play area are a longer term project.</li> </ul>	<ul style="list-style-type: none"> <li>This is an ideal project for a community based fundraising effort that may also include service club involvement and /or sponsorships.</li> </ul>	<ul style="list-style-type: none"> <li>Requires additional area maintenance.</li> </ul>
	Water Play Area (Splash Pad) at Centennial Park	HIGH	\$285,000	<ul style="list-style-type: none"> <li>Alternative water treatment methods may be considered to minimize strain on sewage lagoon.</li> <li>Options: waste water may be re-used for irrigation, or water quality treatment on-site using natural systems (e.g., wetland).</li> </ul>	<ul style="list-style-type: none"> <li>This is an ideal project for a community based fundraising effort that will require additional funding from senior levels of government to support community wide economic development and quality of life objectives.</li> </ul>	<ul style="list-style-type: none"> <li>Enhancements to Centennial Park may require hiring seasonal municipal staff (i.e., maintenance, concession, rental / outfitter stand, etc.).</li> </ul>
	Beach Volleyball Area	MEDIUM	Included in Beach Improvements	<ul style="list-style-type: none"> <li>Part of the beach enhancement project.</li> <li>Potential for regional beach volleyball tournaments and promotions.</li> </ul>	<ul style="list-style-type: none"> <li>Consider removable net costs as a recreation department operational cost.</li> <li>Potential to partner with a beach volleyball club (existing or future).</li> </ul>	<ul style="list-style-type: none"> <li>Requires raking and grooming as well as storage of nets on a daily basis during the season.</li> </ul>
	Other Centennial Park Improvements	MEDIUM	\$490,000	<ul style="list-style-type: none"> <li>This includes all park walkways, specialty paving, gazebo, naturalized plantings, trees, facility building upgrades, viewing platform, site furniture, ramp improvements, and stair and railing improvements.</li> </ul>	<ul style="list-style-type: none"> <li>Assistance will be required.</li> <li>Small sponsorship opportunities exist (i.e., memorial bench, tree, gazebo, etc.).</li> </ul>	<ul style="list-style-type: none"> <li>Requires additional area maintenance.</li> </ul>

DESIGN PRINCIPLE	PROJECT	PRIORITY LEVEL	CAPITAL COSTS	ISSUES + IMPACTS	POTENTIAL FUNDING SOURCES	MANAGEMENT REQUIREMENTS
<i>Identify and Promote Economic Development Opportunities</i>	Marina Development (i.e. docks, service areas, etc.)	MEDIUM	To be determined	<ul style="list-style-type: none"> <li>Private sector business opportunity.</li> </ul>	<ul style="list-style-type: none"> <li>Capital project costs may be eligible for funding assistance.</li> </ul>	<ul style="list-style-type: none"> <li>Formal agreement required between Municipality and private operator.</li> </ul>
	Potential Downtown Redevelopment Sites	On-going	To be determined	<ul style="list-style-type: none"> <li>Several privately owned sites are identified within the Downtown Waterfront Area.</li> <li>The public sector investments in enhanced infrastructure carried out by the Municipality demonstrate a strong commitment to the Downtown Waterfront Area and encourage existing and future owners to consider private sector investment and redevelopment.</li> </ul>	<ul style="list-style-type: none"> <li>The Municipality may incur costs if it defers or waives development application fees or provides matching funding to the property owner through the CIP or other mechanism.</li> </ul>	<ul style="list-style-type: none"> <li>As these properties come up for sale, the Municipality may support redevelopment through the planning approval process and /or the CIP initiatives.</li> </ul>
	Private Sector Property Improvements (e.g. Façade, parking lots, etc.)	On-going	To be determined	<ul style="list-style-type: none"> <li>These opportunities are enabled through the CIP.</li> </ul>	<ul style="list-style-type: none"> <li>Funding through CIP.</li> </ul>	<ul style="list-style-type: none"> <li>CIP must be formally adopted by Council and administered by the Municipality on an on-going basis.</li> </ul>

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## 5.5 Order of Magnitude Cost Estimates

### 5.5.1 Signature Parkette, Waterfront Gateway and Marina

Development Area/Item	Quantity	Unit	Unit Cost	Total
<b>1.0 Signage/Identity/Gateways</b>				
1.1 Core Area Gateway	1	ea	\$ 15,000.00	\$15,000.00
1.2 Minor Gateway	3	ea	\$ 10,000.00	\$30,000.00
1.3 Directional / Identity Signage	4	ea	\$ 5,000.00	\$20,000.00
1.2 Interpretive Signage	6	ea	\$ 2,500.00	\$15,000.00
			Area Total	<b>\$80,000.00</b>
<b>2.0 Streetscape Works Signature Parkette (Landsdowne and Main Street):</b>				
2.1 On-street Parking (12 spaces)				
2.1.1 HD Asphalt	275	m <sup>2</sup>	\$ 50.00	\$13,750.00
2.1.2 P.I.P Concrete Curb	350	lm	\$ 120.00	\$42,000.00
2.2 StreetPrint Colour/Impressed Asphalt				
2.2.1 Intersection Design	165	m <sup>2</sup>	\$ 100.00	\$16,500.00
2.2.2 Crosswalks	100	m <sup>2</sup>	\$ 75.00	\$7,500.00
2.3 Precast Pedestrian Paving	800	m <sup>2</sup>	\$ 100.00	\$80,000.00
2.4 P.I.P Concrete Colour Concrete Paving	250	m <sup>2</sup>	\$ 120.00	\$30,000.00
2.5 Site Furniture				
2.5.1 Benches	12	ea	\$ 1,750.00	\$21,000.00
2.5.2 Trash/Recycle	2	ea	\$ 1,250.00	\$2,500.00
2.5.3 Bike Storage	2	ea	\$ 1,000.00	\$2,000.00
2.5.4 Planters	75	lm	\$ 400.00	\$30,000.00
2.6 Lighting	8	ea	\$ 5,000.00	\$40,000.00
2.7 Banners	16	ea	\$ 250.00	\$4,000.00
2.8 Trees	24	ea	\$ 500.00	\$12,000.00
			Area Total	<b>\$301,250.00</b>
<b>3.0 Streetscape Works Waterfront Gateway:</b>				
3.1 Parking Area				
3.1.1 Granular Surface Parking Area	1100	m <sup>2</sup>	\$ 35.00	\$38,500.00
3.1.2 Lighting	6	ea	\$ 5,000.00	\$30,000.00
3.1.3 Parking Island/Naturalized Planting	90	m <sup>2</sup>	\$ 40.00	\$3,600.00
3.2 P.I.P Concrete Walkway	420	lm	\$ 75.00	\$31,500.00
3.3 Pedestrian Node				
3.3.1 StreetPrint Colour/Impressed Asphalt	60	m <sup>2</sup>	\$ 100.00	\$6,000.00
3.3.2 Precast Pedestrian Paving	220	m <sup>2</sup>	\$ 100.00	\$22,000.00

3.3.3 Benches	6	ea	\$ 1,750.00	\$10,500.00
3.3.4 Lighting	4	ea	\$ 5,000.00	\$20,000.00
3.4 Trees	28	ea	\$ 500.00	\$14,000.00
			Area Total	<b>\$176,100.00</b>

### 4.0 Marina Improvements:

4.1 Pier Improvements/Shoreline Protection (Baird)		lump sum	\$ 2,000,000.00	\$2,000,000.00
4.2 Marina Docking	167	lm	\$ 1,500.00	\$250,500.00
4.3 Boat Launch Improvements	1	ea	\$ 80,000.00	\$80,000.00
4.4 Rest Nodes (2 @300m <sup>2</sup> )	600	m <sup>2</sup>	\$ 750.00	\$450,000.00
4.5 Shade Structures	2	ea	\$ 8,500.00	\$17,000.00
4.6 Benches	8	ea	\$ 1,750.00	\$14,000.00
			Area Total	<b>\$2,811,500.00</b>

### 5.0 Park Improvements:

5.1 Waterfront Promenade				
5.1.1 Boardwalk	1000	m <sup>2</sup>	\$ 450.00	\$450,000.00
5.1.2 Precast Pedestrian Paving	500	m <sup>2</sup>	\$ 100.00	\$50,000.00
5.1.3 Urban Beach (inc. Sand/base)	300	m <sup>2</sup>	\$ 75.00	\$22,500.00
5.2 Bollard Lighting	15	ea	\$ 1,750.00	\$26,250.00
5.3 Facilities Building Concessions/Picnic Shelter				
5.3.1 Building	1	ea	\$ 500,000.00	\$500,000.00
5.3.2 Precast Pedestrian Paving	300	m <sup>2</sup>	\$ 100.00	\$30,000.00
5.4 Gazebo (@28' diameter)	2	ea	\$ 45,000.00	\$90,000.00
5.5 Site Furniture				
5.5.1 Benches	15	ea	\$ 1,750.00	\$26,250.00
5.5.2 Cluster Seating Picnic tables	12	ea	\$ 2,000.00	\$24,000.00
5.5.3 Trash/Recycle	5	ea	\$ 1,250.00	\$6,250.00
5.5.4 Bike Storage	4	ea	\$ 1,000.00	\$4,000.00
5.6 Naturalized Planting Areas	1000	m <sup>2</sup>	\$ 20.00	\$20,000.00
5.7 Trees	28	ea	\$ 500.00	\$14,000.00
			Area Total	<b>\$1,263,250.00</b>

Sub-Total	<b>\$4,632,100.00</b>
Estimated Contingency + Soft Costs (25%)	\$1,158,025.00
Total	<b>\$5,790,125.00</b>
HST (@13%)	\$752,716.25
TOTAL	<b>\$6,542,841.25</b>

5.5.2 Centennial Park

	Development Area/Item	Quantity	Unit	Unit Cost	Total
<b>1.0</b>	<b>Streetscape Works:</b>				
1.1	On-street Parking (12 spaces)				
1.1.1	HD Asphalt	250	m <sup>2</sup>	\$ 50.00	\$12,500.00
1.1.2	P.I.P Concrete Curb	80	lm	\$ 120.00	\$9,600.00
1.2	Crosswalks (Colour Concrete)	150	m <sup>2</sup>	\$ 150.00	\$22,500.00
1.3	Lighting	30	ea	\$ 5,000.00	\$150,000.00
1.4	Banners	30	ea	\$ 250.00	\$7,500.00
1.5	Trees	77	ea	\$ 500.00	\$38,500.00
				Area Total	<b>\$218,500.00</b>
<b>2.0</b>	<b>Parking Area and Boat Launch Access</b>				
2.1	Granular Surface (incl. entrances)	4500	m <sup>2</sup>	\$ 35.00	\$157,500.00
2.2	Culvert Bridge	2	ea	\$ 15,000.00	\$30,000.00
2.2.1	P.I.P Concrete Walls	45	m	\$ 600.00	\$27,000.00
2.3	Lighting	15	ea	\$ 5,000.00	\$75,000.00
2.4	Boat Launch Improvements	1	ea	\$ 30,000.00	\$30,000.00
2.5	Naturalized Planting (@perimeter)	1000	m <sup>2</sup>	\$ 20.00	\$20,000.00
2.6	Parking Island Naturalized planting	280	m <sup>2</sup>	\$ 40.00	\$11,200.00
2.7	Trees	14	ea	\$ 500.00	\$7,000.00
				Area Total	<b>\$170,200.00</b>
<b>3.0</b>	<b>Beach Improvements:</b>				
3.1	Regrading (inc.new sand) (@1500m x 0.6m)	900	m <sup>3</sup>	\$ 25.00	\$22,500.00
3.2	Armourstone Retaining Wall	90	lm	\$ 200.00	\$18,000.00
3.3	General clean-up	1	ea	\$ 5,000.00	\$5,000.00
				Area Total	<b>\$45,500.00</b>

<b>4.0</b>	<b>Park Improvements:</b>				
4.1	Promenade	1000	m <sup>2</sup>	\$ 150.00	\$150,000.00
4.2	Asphalt Walkways	850	m <sup>2</sup>	\$ 35.00	\$29,750.00
4.3	Specialty Paving				
4.3.1	Gazebo @Play Area	200	m <sup>2</sup>	\$ 150.00	\$30,000.00
4.3.2	Gazebo @Amphitheatre	165	m <sup>2</sup>	\$ 150.00	\$24,750.00
4.4	Play Area				
4.4.1	Junior/Senior Play Equipment/Swings	1	ea	\$ 75,000.00	\$75,000.00
4.4.2	Resilient Play Surface/Sand	1000	m <sup>2</sup>	\$ 50.00	\$50,000.00
4.4.3	Armourstone Wall	95	lm	\$ 100.00	\$9,500.00
4.4.4	Gazebo (@28' diameter)	1	ea	\$ 45,000.00	\$45,000.00
4.5	Water Play Area				
4.5.1	Splash Pad (inc.controls, water connection)	1	ea	\$ 200,000.00	\$200,000.00
4.5.2	Rubber Surface	165	m <sup>2</sup>	\$ 150.00	\$24,750.00
4.6	Naturalized Planting Areas	125	m <sup>2</sup>	\$ 20.00	\$2,500.00
4.7	Trees	6	ea	\$ 500.00	\$3,000.00
				Area Total	<b>\$644,250.00</b>

<b>5.0</b>	<b>Other Site Improvements:</b>				
5.1	Facility Building Upgrade	1	ea	\$ 35,000.00	\$35,000.00
5.2	Look-out/Viewing Platform	3	ea	\$ 15,000.00	\$45,000.00
5.3	Site Furniture				
5.3.1	Benches	10	ea	\$ 1,750.00	\$17,500.00
5.3.2	Trash/Recycle	4	ea	\$ 1,250.00	\$5,000.00
5.3.3	Bike Storage	3	ea	\$ 1,000.00	\$3,000.00
5.4	Amphitheatre Gazebo (@24' diameter)	1	ea	\$ 25,000.00	\$25,000.00
5.5	Ramp Improvements	50	m <sup>2</sup>	\$ 250.00	\$12,500.00
5.6	Stair and railing Improvements	1	ea	\$ 15,000.00	\$15,000.00
				Area Total	<b>\$158,000.00</b>

Sub-Total	<b>\$1,236,450.00</b>
Estimated Contingency + Soft Costs (25%)	\$309,112.50
<b>Total</b>	<b>\$1,545,562.50</b>
HST (@13%)	\$200,923.13
<b>Total</b>	<b>\$1,746,485.63</b>



# implementation strategy

## 5.5.3 Streetscape Works - Lansdowne and Main Streets

	Development Area/Item	Quantity	Unit	Unit Cost	Total
<b>1.0</b>	<b>Lansdowne Street (Waterfront to Highway 94)</b>				
1.1	Streetscape Works Includes 2m. wide sidewalks on both sides with decorative paving at corners, street trees and planters (including underground drainage, grates, soil amendments, etc), decorative street lighting (@ 15-20 m O.C.), and street furniture (benches, trash receptacles, bike loops).	820	lm	\$ 2,350.00	\$1,927,000.00
				Area Total	<b>\$1,927,000.00</b>
<b>2.0</b>	<b>Main Street (Park Street to Rock Street)</b>				
2.1	Streetscape Works Includes 2m. wide sidewalks on both sides with decorative paving at corners, street trees and planters (including underground drainage, grates, soil amendments, etc), decorative street lighting (@ 15-20 m O.C.), and street furniture (benches, trash receptacles, bike loops).	1225	lm	\$2,350.00	\$2,878,750.00
				Area Total	<b>\$2,878,750.00</b>
				Subtotal	<b>\$4,805,750.00</b>
				H.S.T. (13%)	\$624,747.50
				<b>TOTAL</b>	<b>\$5,430,497.50</b>

\*25% Estimated contingency + soft costs included in unit cost.

# 6.0

## sustainability considerations





## 6.1 Sustainability Criteria and Performance Measures

Callander's Downtown Waterfront Area will further evolve as an exciting place to live, work and play. Encouraging development that is environmentally sustainable is an important element to achieve this vision. There are a number of performance measures that can be utilized by the Municipality of Callander to ensure that any future development adheres to the overall objective of sustainability.

The following guidelines and benchmarks serve as incremental steps to help transform traditional land development and management practices toward sustainability. These guidelines are meant to encourage site designers and developers to allow natural and built systems to work together to improve the long-term health of the environment and of socially and economically viable communities.

The measures listed below have been adapted from the criteria established by the American Society of Landscape Architects Sustainable Sites Initiative (2009) and are intended to supplement the Canada Green Building Council's LEED Rating System. These measures are as follows:

### **Site Selection Criteria**

- Protect floodplain functions and preserve wetlands;
- Ensure the preservation of threatened or endangered species and their habitats by avoiding development of these areas;
- Select brownfields or greyfields for redevelopment to reduce pressure on undeveloped land;
- Encourage site development within existing community to reduce pollution and development impacts, support local economy and improve human health;
- Select sites that encourage non-motorized transportation and the use of public transit;

### **Pre-Design Assessment and Planning Criteria**

- Conduct a pre-design site assessment and explore opportunities for site sustainability;
- Use a multidisciplinary team to collaborate on the design, construction and maintenance of the site in an integrated process;
- Engage users and other stakeholders in site design process;
- Reduce the use of potable water for landscape irrigation after plant establishment;
- Encourage alternative irrigation methods and water conservation strategies;
- Preserve and enhance riparian, wetland, and shoreline buffers to improve flood control and water quality;
- Rehabilitate the ecosystem functions and values of lost streams, wetlands, and shorelines;

### **Site Design Criteria**

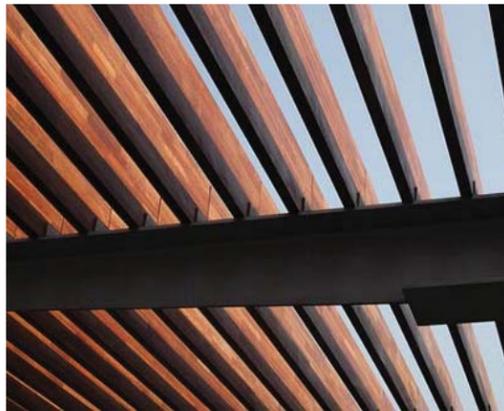
- Manage stormwater on site by restoring the water balance of the site;
- Protect and enhance on-site water resources and receiving water quality;
- Design rainwater / stormwater features to provide a landscape amenity;
- Manage water features to conserve water and other resources;
- Control and manage known invasive plants found on site to limit damage to local ecosystem services;
- Use appropriate, non-invasive plants to improve landscape performance and reduce resource use;
- Develop and communicate a soil management plan prior to construction to limit disturbance and assist soil restoration efforts;
- Minimize soil disturbance in design and construction;
- Identify and preserve all vegetation designated as special status by local, provincial or federal entities;
- Maintain or establish regionally appropriate vegetation biomass to support the ecosystem service benefits provided by vegetation on site;



*Provide outdoor gathering spaces for social interaction and community events*



# sustainability considerations



Utilize regional materials



Interpretive sustainability signage

- Plant appropriate vegetation that is native to the eco-region of the site and preserve and restore existing plant communities native to the eco-region to contribute to regional diversity;
- Place vegetation strategically around buildings to reduce energy consumption and costs associated with indoor climate control;
- Use vegetation and reflective materials to reduce heat islands and minimize effects on microclimate and on human and wildlife habitat;
- Eliminate the use of wood from threatened tree species to minimize negative effects on other ecosystems;
- Utilize existing materials as resources for new site development to reduce costs for new purchased materials;
- Use materials with recycled content to reduce the use of virgin materials and avoid sending useful materials to the landfill;
- Ensure certified lumber is used to encourage exemplary forest management that is environmentally and socially responsible;
- Use regional materials to reduce energy use and support local resources and promote a regional identity;
- Select products in site development that contain reduced amounts of volatile organic compounds (VOCs) to reduce harmful health effects associated with air pollution;
- Support sustainable practices in plant production and materials manufacturing by purchasing plants and materials from providers who reduce resource consumption and waste;

## **Human Health and Well-Being Criteria**

- Promote equitable site development by ensuring that project provides economic or social benefits to local community during construction and end use;
- Interpret on-site features and processes to promote the understanding of sustainability in ways that positively influence user behaviour on site and beyond;
- Protect and maintain cultural and historical locations, attributes and artifacts to enhance a site's sense of place and meaning;
- Provide for optimum site accessibility, safety and wayfinding of outdoor spaces;
- Provide on-site opportunities that encourage outdoor physical activity to improve human health;
- Provide visual and physical connections to the outdoors to optimize the mental health benefits of site users;
- Provide outdoor gathering spaces of various sizes and orientations to accommodate groups to build community and improve social ties;
- Reduce light pollution by minimizing light trespass on site to reduce sky-glow, increase nighttime visibility and minimize negative effects on nocturnal environments and human health;

**Construction Criteria**

- Prevent the discharge of construction site pollutants and materials to protect receiving waters, air quality and public safety;
- Restore soils disturbed during construction in all areas that will be re-vegetated and areas of previously disturbed topsoils and subsoils to rebuild site's ability to support healthy plants, biological communities, water storage and infiltration;
- Divert construction and demolition materials generated by site development from disposal;
- Reuse or recycle vegetation, rocks, and soil generated during construction to achieve a net zero waste site;
- Use construction equipment that minimizes the generation of greenhouse gas emissions and localized air pollutants;

**Monitoring and Innovation Criteria**

- Monitor and document sustainable design practices to evaluate their performance over time and improve the body of knowledge on long-term site sustainability;
- Encourage and reward innovative sustainable practices for exceptional performance and/or innovative performance in sustainable sites;

**Operations and Maintenance Criteria**

- Prepare a site maintenance plan outlining long-term strategies and short-term actions to achieve sustainable maintenance goals;
- Provide for storage and collection of recyclables in outdoor areas to reduce waste generation;
- Select energy-efficient outdoor fixtures and equipment to reduce energy consumption and costs;
- Use electricity from renewable sources to reduce greenhouse gas emissions associated with site operations and minimize air pollution, habitat destruction, and pollution from fossil fuel-based energy production;
- Minimize generation of greenhouse gases and exposure to localized air pollutants during landscape maintenance activities;
- Promote the use of vehicles that have reduced emissions and/or high fuel efficiency to reduce pollution and land development impacts from automobile use;

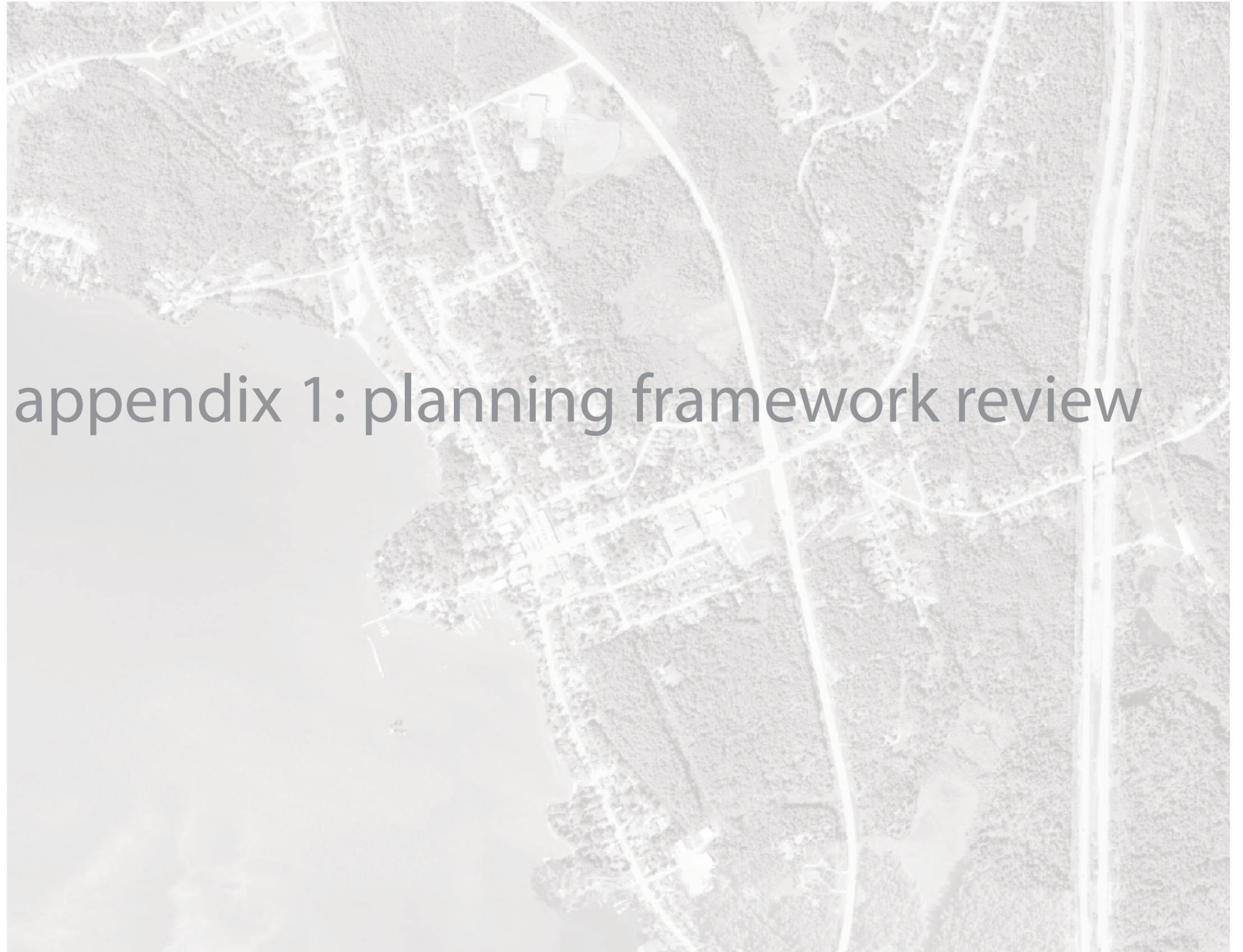


*Integrate stormwater features into landscape design*



A1

appendix 1: planning framework review





## A1.1 Policy Framework

### ***Places to Grow - Growth Plan for Northern Ontario (2011)***

The Places to Grow - Growth Plan for Northern Ontario was released on March 4, 2011 by the Province of Ontario (Ministry of Infrastructure and Ministry of Northern Development, Mines and Forestry). This Plan provides a framework to guide decision-making and investment planning in Northern Ontario for the next 25 years. The Municipality of Callander lies within the southern portion of Northern Ontario, just south of North Bay.

The vision for Northern Ontario is that of a highly skilled, educated, healthy and prosperous region that has modern infrastructure and world-class resources and technology.

To achieve that vision, collaborative efforts should be focused on six guiding principles:

1. Creating a highly productive region, with a diverse, globally competitive economy that offers a range of career opportunities for all residents.
2. Developing a highly educated and skilled workforce to support an evolving knowledge-based economy and excellence in the trades.
3. Partnering with Aboriginal peoples to increase educational and employment opportunities.
4. Delivering a complete network of transportation, energy, communications, social and learning infrastructure to support strong, vibrant communities.
5. Demonstrating leadership in sustainable growth and environmental management.
6. Establishing innovative partnerships to maximize resources and ensure this Plan achieves its ambitious vision and is fiscally sustainable.

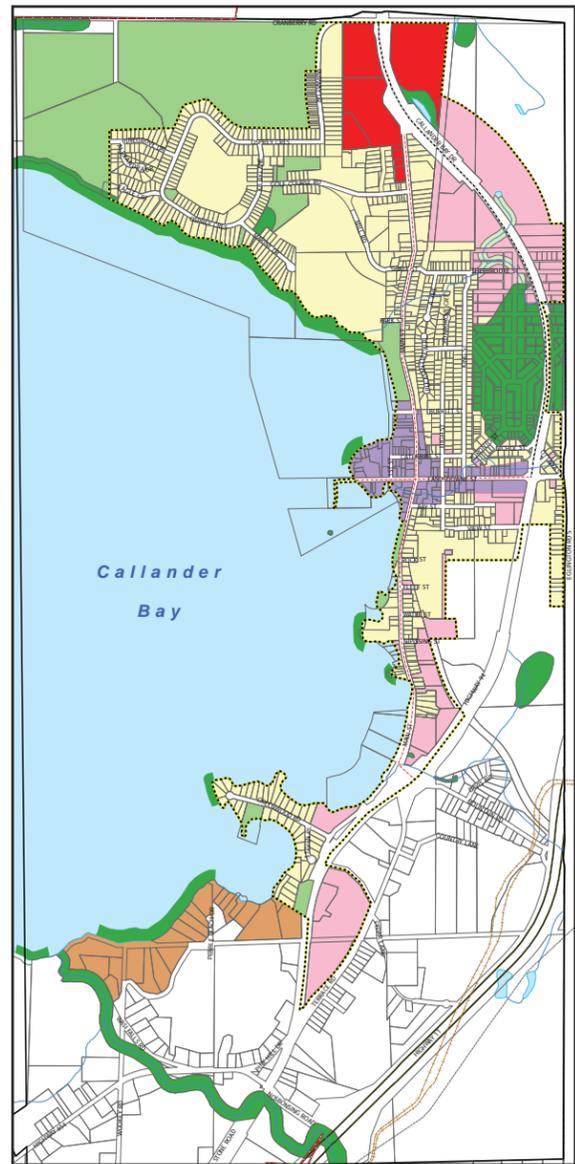
The preamble to the Infrastructure section (Section 5) states that wastewater infrastructure (one item in a list of other elements) is important for economic growth. As well, Section 5.2.2 indicates that the Province will give priority to infrastructure investments that support the policies in the plan. Policies are meant to promote economic prosperity, sound environmental stewardship and strong, sustainable communities that offer a high quality of life.

Upgrading the sewage infrastructure in Callander appears to align with the policies of promoting economic prosperity, as wastewater infrastructure is considered one building block for economic growth (Section 5 preamble). Section 2.3.10 (Tourism) also supports “investing in strategic public infrastructure and the provincial parks system to improve the competitiveness of the tourism industry and enhance the visitor experience.”



*Northern Ontario Growth Plan Context Map*  
(Source: Growth Plan for Northern Ontario, Government of Ontario, 2011)

# appendix 1: planning framework review



Extent of Callander Urban Area as defined in Official Plan

## ***Municipality of Callander Official Plan Update (2011)***

The Official Plan Update for the Municipality of Callander was approved in August 2011, completed by Meridian Planning Consultants. This Official Plan (OP) is meant to “manage land use change in a manner that has the greatest positive impact on the Municipality while ensuring the sustainability of the environment and the character of the community”. A number of objectives are identified within this plan including those related to growth management, natural environment, economic development, waterfront areas, cultural heritage, services and transportation, resources and aggregates, energy conservation, sustainability, leisure and recreation, accessibility, and urban design. The OP outlines a Land Use vision for the future of the Municipality, which is based on a number of principles:

- Protection and enhancement of natural environment and heritage features.
- Management of growth through allocating servicing capacity and development phasing.
- Management of shoreline development and protecting the lakeshore environment.
- Protection of the rural and resource lands from incompatible development that may preclude rural and resource related land uses.
- Maintaining the character of the community.

The Official Plan identifies the Callander Settlement Area as the only settlement area within the Municipality, and where a majority of the anticipated population growth is expected to locate. The OP sets out a number of objectives related to the Callander Settlement Area designation that are important for the development of the Downtown Waterfront Revitalization Master Design Strategy. These include but are not limited to the following:

- Maintain and enhance the character and identity of the Settlement Area;
- Retain the Downtown Area as main focal point of community and the dominant service and commercial centre within the Municipality;
- Preserve and enhance the vitality of traditional main street character and encourage a mix of residential, commercial and institutional uses in the Downtown Area;
- Encourage the development of attractive gateways to the Settlement Area;

- Encourage a range of housing types; and,
- Encourage commercial development to provide a full range of goods and services to residents.

Within the Callander Settlement Area, the Downtown Area designation is centered on the intersection of Lansdowne Street and Main Street, the boundaries of which are consistent with the study area boundaries for the Downtown Waterfront Revitalization Master Design Strategy. The Official Plan states that “the predominant use of land in the Downtown Area designation shall be a mix of commercial and residential uses”. A number of development policies are outlined in the Official Plan relating to the Downtown Area:

- Mixed use buildings are encouraged in order to achieve a diverse mix of land uses and higher densities.
- All new development or redevelopment shall reflect the scale, density and massing of surrounding structures, be limited in height to three storeys and incorporate and improve upon the existing historical structures.
- Development abutting the waterfront shall encourage public access to the shoreline, provide connectivity between public lands and preserve views to the lake.

As well, a number of urban design principles are outlined pertaining to the Downtown Area, including:

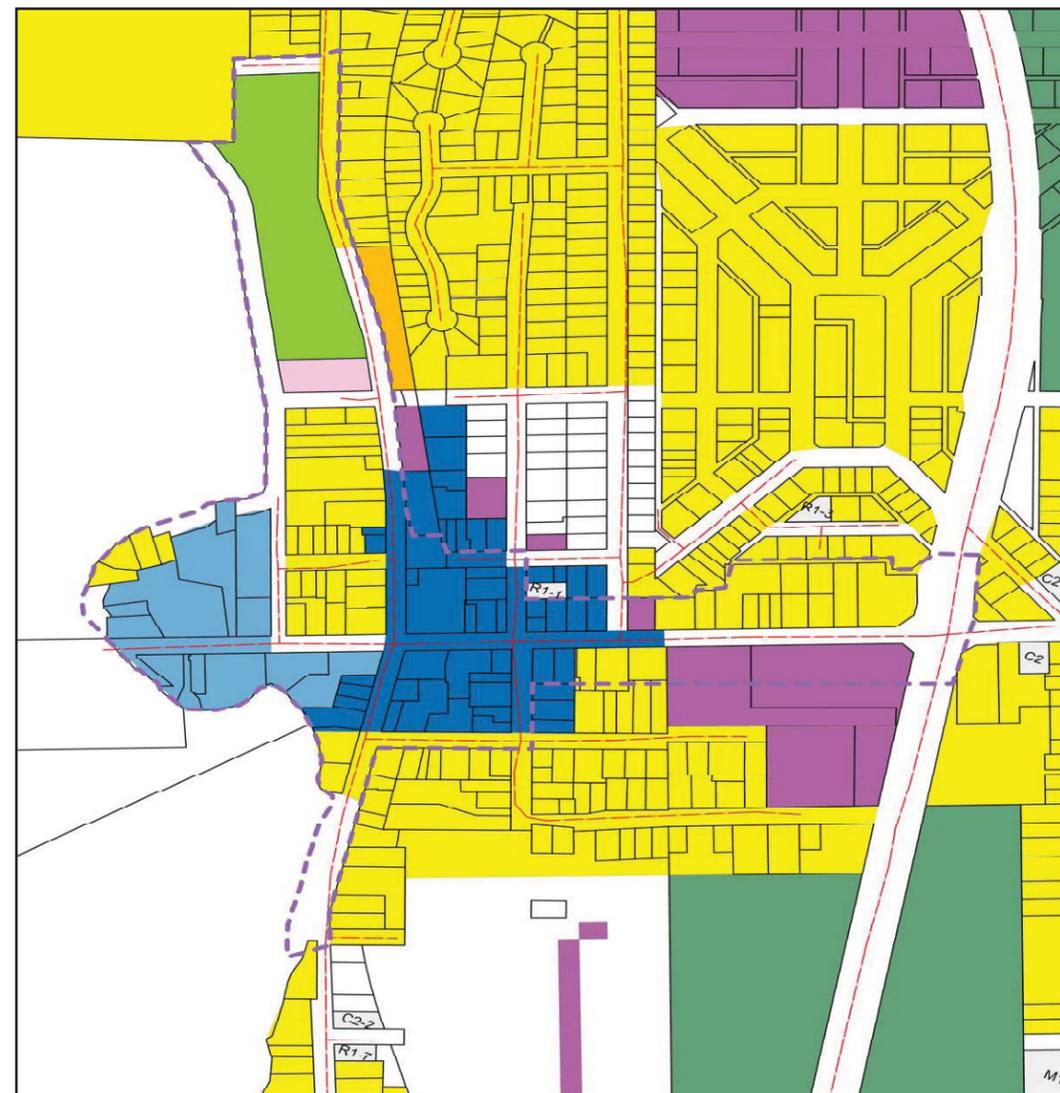
- Active street life is encouraged through the development of municipal squares, parkettes, street related buildings, outdoor patios, etc.
- Enhance pedestrian environment through the provision of benches, awnings, pedestrian-scale lighting, streetscape amenities, grade-level building entrances, and high quality architectural design.
- Parking areas to be sited to the side or rear of buildings to promote a pedestrian environment on the street.
- Site buildings to create a continuous façade, defining the edge of the street.
- Enhance public safety through the design and siting of buildings and open space.

**Municipality of Callander Zoning By-Law 2003-857 (2003)**

The Municipality of Callander Zoning By-law was prepared by Meridian Planning Consultants in June 2003 and amended in September 2003. The Zoning By-law applies to all lands within the geographic limits of the Municipality of Callander. The Downtown Area's most prevalent designation is C1 – General Commercial which permits accessory dwelling units, financial institutions, offices, hotel and motel, medical clinic, recreation facility, service shops, hospital, public park, restaurant, retail store, school and day nursery. Many of the properties zoned C1 are located along Lansdowne Street and Main Street.

Other designations that are present within the study area include R1 – Residential, permitting home occupation, duplex, semi-detached, single detached, and link home dwellings, located along the eastern portion of Lansdowne Street within the study area, and the west side of Main Street north of Lansdowne Street; CR – Resort Commercial which permits camping establishment, hotels and motels, private and public camps, marina, recreation facility, tourist establishment. This designation is found surrounding the terminus of Lansdowne Street at Callander Bay and the Callander Wharf. I – Institutional is also found within the study area permitting hospitals, medical offices, municipal recreation facilities and offices, place of worship, service club and schools. These lands are located mostly on Lansdowne Street at Highway #94, with some Institutional designations located on Main Street, in particular the Medical Building and the Municipal Offices.

The OS – Open Space designation is also located within the study area, along the shoreline at the northern and southern study boundary. This land use designation permits community centre, golf course and driving range, recreation facility, private or public park, passive recreation and yacht club. Enhancing the waterfront parks for public enjoyment is important to the overall vibrancy and attractiveness of the Downtown Waterfront area. It is important to note that this Zoning By-Law has not been updated to reflect the recently approved Official Plan (2011).



Downtown Waterfront Area Zoning Designations

*“Enhancing the waterfront parks for public enjoyment is important to the overall vibrancy and attractiveness of the Downtown Waterfront area.”*

LEGEND	
<span style="color: yellow;">■</span>	R1 - Residential
<span style="color: orange;">■</span>	RM-3 - Multiple Residential
<span style="color: blue;">■</span>	C1 - General Commercial
<span style="color: lightblue;">■</span>	CR - Resort Commercial
<span style="color: purple;">■</span>	I - Institutional
<span style="color: pink;">■</span>	I-2 - Institutional Exception 1
<span style="color: green;">■</span>	OS - Open Space
<span style="color: darkgreen;">■</span>	RU - Rural



# appendix 1: planning framework review

*“The purpose of this Strategic Plan is to develop a strategy that can build upon the existing natural and community resources.”*

## ***Municipality of Callander Strategic Plan (Revised 2008)***

The North Himsforth Strategic Plan was completed in June 2002 by Meridian Planning Consultants and The Planning Partnership, and was revised in 2008 to reflect the area’s new name of Municipality of Callander. This plan is intended to guide Council, Committees of Council, community organizations and residents on future decisions and actions. The purpose of this Strategic Plan is to develop a strategy that can build upon the existing natural and community resources. The mission statement of the Strategic Plan is as follows:

“We will nurture the small town community pride and spirit and preserve the quality of life while developing the community in a manner that will make the most efficient use of existing infrastructure and resources.”

Objectives defined within the Strategic Plan include:

- Maintain, protect and enhance the natural features of the Town while improving access where it is sustainable;
- Beautify the community;
- Celebrate the diverse cultural history of the Town;
- Provide more facilities, services and amenities;
- Improve communications regarding facilities, services, amenities and attractions in the Town;
- Encourage local business to grow and diversify; and,
- Provide new facilities within the financial means of the municipality, and making the most efficient use of municipal services.

A number of projects are identified within the Strategic Plan that would enhance the Municipality of Callander and a number of priority actions for implementation are suggested. The priority actions include:

- Change the Name of the Town (this was completed in 2003)
- Marketing
- Establish a Town Business Improvement Area
- Downtown Beautification
- Trail Development
- Improve Signage – Develop Gateways (undertook a Corporate Brand Identity Signage Program in 2005)
- Consolidate Shoreline Lands
- Improve the Appearance and Use of the Community Centre in Callander
- Significant Tourist Destination Attraction
- Municipal Docking Facility

## A1.2 Economic Framework

### **Community Improvement Plan for Central Callander (1991)**

The Community Improvement Plan for Central Callander was prepared by McNeely-Tunnock Ltd. in November, 1991. This is an historical document prepared for the Township of North Himsforth and it recommended the following infrastructure investments:

- Reconstruction of Main Street
- Reconstruction of sidewalks on Main and Lansdowne streets
- Improve streetscaping along Main Street with regard to street lights, street furniture and landscaping
- Improve on-street parking conditions
- Improve the park infrastructure in Centennial, Memorial Tree and Sunbeam parks

The plan was established under previous provisions of the Ontario Planning Act and is outdated in terms of its legislative controls which have seen an expansion of the provisions related to Community Improvement Plans (CIPs).

Based on this review we recommend the re-tasking of the CIP policies of the plan as follows:

1. Develop a new rationale and mandate for Community Improvement Plan policies to reflect the provisions and priorities of current municipal and regional planning policy (as well as the newly released Growth Plan for Northern Ontario).
2. Confirm boundaries of the Community Improvement Project Area (CIPA), including the identification of target zones within this to reflect the efforts to revitalize downtown specifically.
3. Identify a suite of programs to provide incentives to the private sector development.
4. Identify a range of public realm improvements as the core element of the design master plan for the Waterfront and Downtown Area.
5. Identify performance measures relating to implementation of the plan.

### **Proposed Economic Development Strategy (Draft) (2010)**

The Draft Proposed Economic Development Strategy was prepared by the Economic Development Department at the Municipality of Callander in October, 2010. This document points out that Callander has approximately 165 businesses and organizations, however many residents work and conduct their business in the nearby City of North Bay. The strategy also provides a brief overview of the policy framework around economic development in Callander:

#### **Strategic Plans (2002 and 2008)**

- Encourage local business to grow and diversify
- Establish a Business Improvement Area (BIA)
- Encourage new industrial and commercial development as long as it is compatible with environment and land uses

#### **Official Plan (2011)**

- Establish, maintain and enhance employment areas that provide a range of job opportunities and a broad range of commercial and service facilities geared specifically to meet the needs of residents
- Encourage the development of home based businesses provided the proposed use is compatible with adjacent land uses
- Provide opportunities to improve/enhance the quality of tourist facilities and the variety of tourism related businesses in the Town

Strategies include: targeting zones of action; building community-level institutions for development that would help attract businesses and better facilitate business retention and expansion; outreach efforts to support business attraction, retention and expansion; building partnerships regionally; attracting quality jobs; and exploring public private partnerships (PPP).

Currently, there is no BIA in Callander, but there is a business association that promotes networking opportunities. Creating a BIA as recommended by the Strategic Plans is an important first step.

*“Callander has approximately 165 businesses and organizations, however many residents work and conduct their business in the nearby City of North Bay.”*



# appendix 1: planning framework review

## **Development Charges Background Study (2009)**

The Development Charges Background Study, prepared by Meridian Planning Consultants Inc. in September 2009, provides the rationale for the structuring of new development charges that will be used to finance growth-related capital costs. Over the past 10 years, residential assessment has accounted for an average of 95.68% of total assessment.

Relevant to this waterfront revitalization study, the document states that the Callander library is 1,335 ft<sup>2</sup> in size. Estimates show that it would cost \$150 / ft<sup>2</sup> to build additional library floor space.

The following is a summary of maximum calculated development charge based on 2.5 persons per household.

<i>Charge Area</i>	<i>Development Charge</i>
Rural Area – Dwelling Unit (2.5 household size)	\$4,494.30
Urban Area – Dwelling Unit (2.5 household size)	\$6,596.66

*“The north-south leg of the wharf was severely damaged to the extent that it is unusable and was determined to be beyond economical repair in its present condition.”*

## A1.3 Servicing and Infrastructure Framework

### **Wharf Structural Assessment and Costing (2009)**

The Wharf Structural Assessment and Costing was prepared by Mar-Land Engineering Limited in December 2008. This report provides an overview of the results of the facility evaluations and recommendations for restoration work for the marine wharf facility on Callander waterfront, which suffered significant damage during a storm on May 14, 2009. The north-south leg of the wharf was severely damaged to the extent that it is unusable and was determined to be beyond economical repair in its present condition. The east-west pier was determined to be in good condition, while the replacement of wood decking between cribs should be done during restoration and replacement of the Launch Ramp in its entirety is required due to deteriorating and shifting concrete slabs.

This report makes a number of recommendations for the wharf structure, complete with cost estimates. The preferred restoration option for the North-South pier to make it a usable public access facility is to encapsulate existing pier section in sheet pile / concrete cap structure with minimal demolition. Another option is to convert the North-South pier to a rubble-mound breakwater structure. However, once a breakwater it is unfeasible, both economically and structurally, to revert back to a public access type structure. Cost estimates for the steel sheet pile encapsulation are stated at \$1,850,000 (\$9,000 per metre) and for the rubble-mound option are \$2,020,000 (\$10,000 per metre).

Further recommendations include that remedial work to the East-West pier be limited to replacement of the timber decking only between cribs. New curbs and mooring devices should also be installed with the new decking. Remedial work for the East-West pier is budgeted at \$10,000.

It is recommended that the Launch Ramp be demolished and rebuilt; while consideration for a slightly wider ramp in the new design is recommended. The demolition and replacement of the Launch Ramp is budgeted at \$60,000.

The Consulting Team is in general agreement with the findings of this report. Cost estimates are approximate, but reasonable for the recommended work. It should be stressed that the North-South portion of the wharf is not structurally sound and recommends that the Municipality continue to maintain access barriers to the North-South portion of the wharf for public safety.

Other remarks regarding this report include:

- Continued failures and breaches of the North-South portions of the wharf will allow wave transmission into the lee of the breakwater, making mooring and docking facilities less viable.
- The bed conditions are unknown, including the soil parameters. Many of the alternatives and their cost estimates will be impacted by the soil conditions. For example, if bedrock is encountered at a shallow depth, the sheet pile option will be more costly as rock toe-pins will be needed. The Mar-Land report recommends geotechnical investigations with the next phase of design study. This recommendation is confirmed by the Consulting Team.
- The existing wharf is within Intake Protection Zones. It is strongly recommended that additional activities associated with wharf development include the water treatment plant operator and the Conservation Authority as stakeholders.
- Maintenance costs have not been provided with the Mar-Land report.
- Beyond the scope of the Mar-Land report, but noted was recommendations for lighting, electrical, and water supply.
- It may be possible to re-use the existing breakwater ballast as core material in the construction of a rubble-mound breakwater. This could reduce the cost of the rubble-mound option.
- There may be additional alternatives available that are not presented in the Mar-Land report.

A full assessment of the Wharf Structure, completed by the Consulting Team, is provided in Appendix 2.

### ***Road Needs Study (2010)***

The Road Needs Study was prepared for the Municipality of Callander by D.M. Wills Associates Limited in June 2010. This study reviews the road system in detail to completely define the road system, identify physical deficiencies and to complete the priority list for road needs. Overall, the study specifies that approximately 48 percent of the municipal road system is in need of rehabilitation to some degree. The study also indicates that during the next 1 to 5 years an additional 36 percent of the road system will also require some degree of rehabilitation.

All roads within the Municipal road system are required in the Methods and Inventory Manual to be studied as separate units. The roads were divided into sections and inventoried according to uniform traffic volume, uniform terrain, uniform physical conditions, and uniform adjacent land. It should be noted that this study only examined the road surface portion of the network, and does not consider other roads needs such as horizontal and vertical alignment, culverts, traffic count and forecasts.

Of the 69.7 kilometres of roads inventoried, a total of 33.9 kilometres were found to be critically deficient within the NOW period. Overall, the roads in need of the most repair are sections of Main Street; while sections of Lansdowne Street are also categorized in the NOW period for repairs. This study identifies approximately \$6.2 million in NOW deficiencies, while deficiencies in the 1 to 5 year time frame are identified at approximately \$4.3 million of additional work.

A five year program for about 25 percent of the road system (in particular the critical care roads) has been developed in conjunction with the Municipality. Highlights of this program include:

- Year 1 (2010): Lansdowne Street, Highway 94 to Lake Nipissing (end).  
Cost: \$227,000
- Year 4 and 5 (2013 & 2014): Main Street, Lansdowne Street to Park Street.  
Cost: \$156,000

# appendix 1: planning framework review

## A1.4 Special Studies

### **Affordable Housing Needs Assessment Study (2009)**

This study, prepared by Planscape in September, 2009, proposes affordable housing unit targets of 59 affordable rental units and 59 affordable ownership units over the next 10 years. These targets are broken down into short, medium and long-term.

Recommended strategies address zoning constraints to affordable housing provision such as: removing one accessory dwelling unit cap in General Commercial and Local Commercial; removing off-street parking requirement for apartments above commercial; and reducing side yard requirements for link home dwellings. The recommendations also suggest ways in which Callander can better facilitate affordable housing production by waiving development, building permit and planning application fees.

*“The paper recommends that 25% of future housing should be row or townhouse development and 15% should be apartment units.”*

### **Growth Management Discussion Paper (2008)**

The Growth Management Discussion Paper was prepared in March, 2008 by Meridian Planning Consultants Inc. Discussing the growth of the community until 2026, the paper documents the issues with servicing constraints and states that there is “very little remaining capacity in the sewage lagoon system in Callander.” Further, the paper concluded that there is “insufficient capacity to service the remaining approved residential units in the municipality.”

In terms of housing demand projections, 400 housing units will need to be built from 2006-2026. After reviewing draft approved and final approved plans of subdivision in 2007, the report states that there appears to be enough dwelling units in the development pipeline to meet projected demand. To encourage a greater range of housing to meet the needs of young families and seniors, the paper recommends that 25% of future housing should be row or townhouse development and 15% should be apartment units. These targets have been incorporated into the 2010 Official Plan.

No additional land is required to accommodate potential residential or employment development. There are 17.9 hectares of vacant designated employment area in Callander.

### **List of Major Employers 2009**

Public Sector	Employees
Municipality of Callander	20
Near North District School Board	20
Nipissing Parry Sound Catholic District School Board	20
Callander Medical Centre	15
Canada Post	8
Private Sector	
Rug-a-roo	20
Osprey Links Golf Club	20
Foodland	19
Comsatec	15

Source: Community Profile, Municipality of Callander, 2007 (taken from 2008 Growth Management Discussion Paper)

**Historical Construction of Employment Lands – Callander (Building Space – m2)**

Year	Industrial	Commercial	Institutional	Total
1996	0	0	0	0
1997		111.5	0	111.5
1998	0	0	0	0
1999	0	0	0	0
2000	278.7	326.9*	0	605.6
2001	185.8	65.8	0	251.6
2002	195	74.32	0	269.32
2003	0	371.5	0	371.5
2004	0	0	0	0
2005	0	0	3,451**	3,451
<b>Total</b>	659.5	950	3,451	5,060.5

Source: Municipality of Callander Building Department (2008 Growth Management Discussion Paper)

\* Commercial development includes rental cottages

\*\* Construction of elementary school in Callander

**Retail Mix**

The retail mix in downtown Callander is made up of an LCBO, coffee shop, restaurants, gas station, grocery store, bar establishment, bank, and pharmacy. Services include a Post Office, public library, and health care centre. Downtown Callander offers a healthy mix of retail and services, but a hardware store is missing. According to stakeholder consultations, the choice and value in the grocery store and LCBO may not always fulfill the needs of resident shoppers. As a result, most shop in North Bay for groceries, using the local grocery store as a top-up instead of a major shop. The selection in the local LCBO is also limited. There may also be opportunities to expand commercial space for professional health services and other services such as tax accounting.

**Corporate Brand Identity Signage Program (2005)**

TWG Communications completed the Corporate Brand Identity Signage Program Report in January 2005. The purpose of this report was to develop a corporate brand identity and signage program for the Municipality of Callander. This report includes actual signage design suggestions (directional, municipal and tourist signage) as well as suggested locations for signage within the town limits. The objectives of the project include enhancing the beautification of the town and enforcing the integrity of the logo, enhance signage to entice tourism, promote the municipality and the brand through directional and municipal signage, and create uniformity within the municipality regarding the logo.

The report contains current signage findings mostly related to the lack of consistent signage at either entrance point into town and lack of consistency regarding directional signage, which was reported to exist in a mixture of sizes and textures.

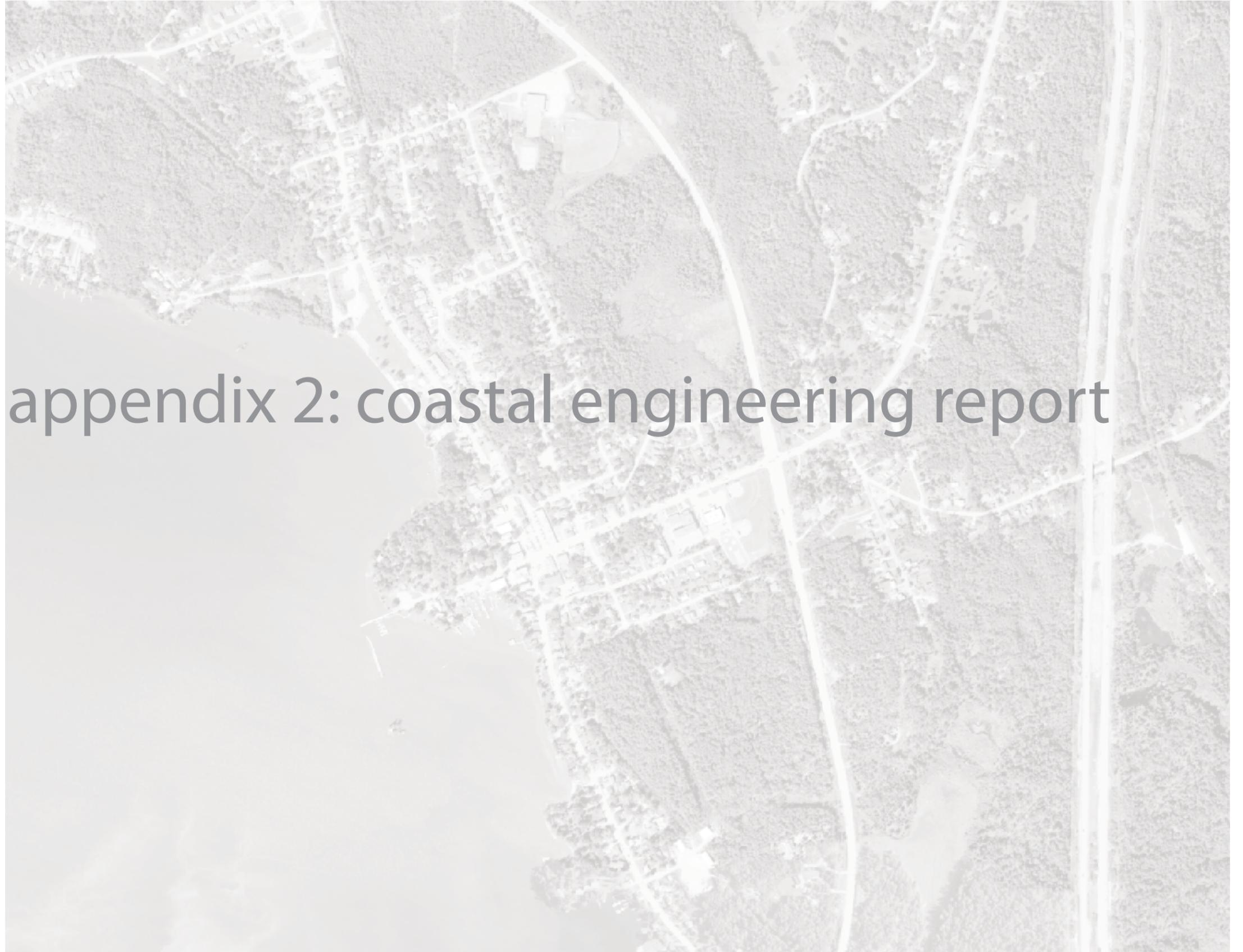
The report specifies a number of recommended changes and enhancements to the signage system in place, proposing a phased approach to implementation. These recommendations are divided into six categories including:

- Large primary highway signage
- Secondary highway signage
- Municipal office signage
- Municipal services/facilities/attractions directional signage
- Street signage
- Banners and beautification signage

Recommendations for each of the categories include design and audience, material recommendations, other considerations and budget estimates.



# A2 appendix 2: coastal engineering report





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### Coastal Engineering Report

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Prepared by

# Baird

W.F. Baird & Associates Coastal Engineers Ltd.

For further information please contact  
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11819.101

*Draft*

## Coastal Engineering Report Downtown Waterfront Revitalization Master Design Strategy and Community Improvement Plan

April 2011  
11819.101



Revision	Date	Status	Comments	Reviewed by	Approved by
0	2011-04-11	Draft	Client review		

This report was prepared by W.F. Baird & Associates Coastal Engineers Ltd. for EDA Collaborative Inc. The material in it reflects the judgment of Baird & Associates in light of the information available to them at the time of preparation. Any use which a Third Party makes of this report, or any reliance on decisions to be made based on it, are the responsibility of such Third Parties. Baird & Associates accepts no responsibility for damages, if any, suffered by any Third Party as a result of decisions made or actions based on this report.



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This report is in draft form for review by EDA Collaborative Inc. It should be treated as confidential until Baird issues the final report.

## 1.0 INTRODUCTION

The Municipality of Callander is undertaking a strategic planning exercise to develop a waterfront and community improvement plan that integrates its existing marine and shoreline assets into the social, cultural and commercial fabric of the downtown Callander community. The process was initiated by the Municipality of Callander, who hired EDA Collaborative Inc. to prepare the strategic plan. Baird & Associates has contracted with EDA Collaborative to provide coastal and marine engineering to the project team. This report provides a summary of the coastal engineering related issues associated with the revitalization of the waterfront.

One of the key features of the waterfront is an L-shaped wharf. The wharf is over a century old and has acted as a focal point for waterfront activities in the Callander Bay area throughout its useful life. Over the past several years, the conditions of the wharf have deteriorated such that portions of the wharf are now closed to pedestrian access due to safety and stability concerns. This report summarizes existing studies completed to date regarding the stability and revitalization of the wharf as well as providing additional insight to alternatives and conceptual costs for the rehabilitation of the wharf.

In the past, the wharf has acted as a port of refuge and marina for vessels operating in Callander Bay. A private non-secured marina operation has developed within the lee of the wharf however these operations are not formalized and are not integrated with the municipal framework. The report provides a base of information for the municipality to review when considering marina proposals in the future.

In addition to the wharf, there are further waterfront resources available in the area with good potential for linkages to the downtown core. This report identifies coastal engineering issues associated with these locations.

## 2.0 REVIEW OF WHARF AND OPTIONS FOR REHABILITATION

This section provides a summary of Baird’s initial thoughts related to the existing wharf and options for rehabilitation. This includes a review of the Mar-Land report, the presentation of additional alternatives, and additional notes for consideration.

An overhead view of the Callander Bay wharf with labels used in this report is provided in Figure 1.



Figure 1. Callander Bay Wharf

**2.1 Review of Mar-Land report**

A review of the Mar-Land Engineering Limited report dated December 2009 (hereafter referred to as *the Mar-Land report*) has been completed. Note that the review was conducted for summary purposes – technical calculations and cost estimates have not been re-calculated.

A summary of the findings is provided in the following bulleted list:

- The East-West portion of the wharf is in relatively good structural condition, with decking repairs and the installation of new mooring hardware and curbs as the recommended actions. These repairs were budgeted at approximately \$10,000.
- The North-South section of the wharf is generally in a state of disrepair, is structurally unsound, and should be demolished or rehabilitated. Two alternatives were presented for the repair of this section of structure, one being the encapsulation of existing material with steel sheet pile, and the other being the removal of the existing structure and the replacement with a rubblemound breakwater. Cost estimates for the steel sheet pile encapsulation are stated at \$1,850,000 (\$9,000 per metre) and for the rubblemound option are \$2,020,000 (\$10,000 per metre).
- The boat ramp immediately North of the East-West section of the wharf is in a state of disrepair and should be demolished and replaced. The Mar-Land report recommends \$60,000 for this replacement.

**2.2 Baird Comments on the Mar-Land report**

Baird is in general agreement with the findings of the Mar-Land report. Cost estimates are approximate, but reasonable for the recommended work. Baird stresses the fact stated in the Mar-Land report that the North-South portion of the wharf is not structurally sound and Baird recommends that the Town continue to maintain access barriers to the North-South portion of the wharf for public safety.

Baird adds the following remarks to the findings of the Mar-Land report:

- Continued failures and breaches of the North-South portions of the wharf will allow wave transmission into the lee of the breakwater, making mooring and docking facilities less viable.
- The bed conditions are unknown, including the soil parameters. Many of the alternatives and their cost estimates will be impacted by the soil conditions. For example, if bedrock is encountered at a shallow depth, the sheet pile option will be more costly as rock toe-pins

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will be needed. The Mar-Land report recommends geotechnical investigations with the next phase of design study. Baird confirms this need.

- The existing wharf is within Intake Protection Zones. It is strongly recommended that additional activities associated with wharf development include the water treatment plant operator and the Conservation Authority as stakeholders.
- Maintenance costs have not been provided with the Mar-Land report. Baird has included an estimate of ongoing maintenance costs in Section 2.4.1 of this report.
- Beyond the scope of the Mar-Land report, but noted was recommendations for lighting, electrical, and water supply. Additional notes on these elements is provided by Baird in Section 2.4.
- It may be possible to re-use the existing breakwater ballast as core material in the construction of a rubblemound breakwater. This could reduce the cost of the rubblemound option.
- Baird believes there may be additional alternatives available that are not presented in the Mar-Land report. Baird has provided this input in Section 2.3 of this report.

## 2.3 Additional Alternatives

Baird confirms that the two alternatives presented in the Mar-Land report are valid alternatives. While it is well beyond the scope of Baird's current agreement to undertake design work for this project, Baird has conducted similar efforts on jobs throughout the world and we have provided some basic notes and schematics from this past experience on the available alternatives in this section of the report. These designs should be treated as conceptual alternatives. **Any elevations, dimensions, or material sizing stated on the schematics is not applicable to Callander Bay.**

Baird has identified three alternatives in addition to the two identified in the Mar-Land report. These are: the installation of a hybrid structure with a rubble structure on the lake side and a steel sheet pile structure on the marina side; a concrete caisson structure; and the placement of rubble on both side of the existing structure combined with the rehabilitation of the existing structure above the low water mark.

### 2.3.1 Hybrid Structure

A hybrid structure is one that allows for the advantages of rubblemound wave energy dissipation on the lake side of the facility while also allowing for public access and the navigation advantages

of a vertical structure on the sheltered harbour side of the structure. From a cost perspective, this would be on a similar order of magnitude as the existing proposals presented in the Mar-Land report. A schematic of a similar design completed by Baird is provided in Figure 2. This type of design would allow for a parapet wall between the deck and the rubblemound feature, which would enable a lower deck elevation – generally considered more desirable for recreational boaters while requiring less material.

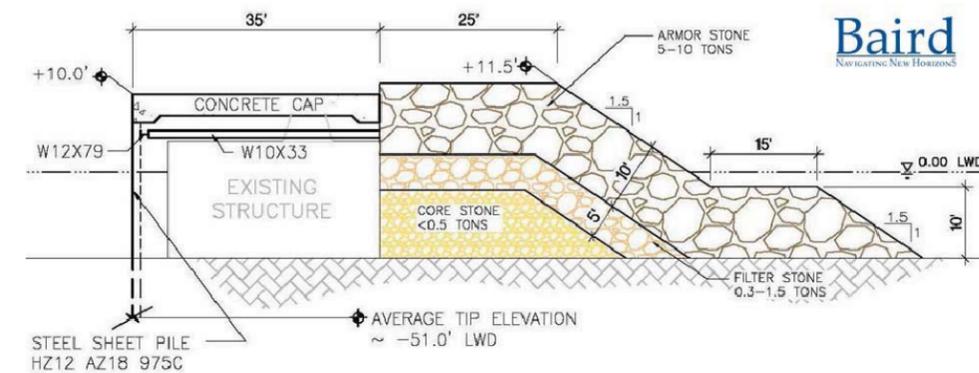


Figure 2. Hybrid structure with rubblemound exposed to lake and steel sheet pile on harbour side (Baird, 2008)

### 2.3.2 Encapsulation with Rubblemound

The existing timber crib structure could be encapsulated with rubble/armour on both sides. A rehabilitated deck could then be used to maintain public access. The cost for this would likely be on the same order of magnitude as the pure rubblemound structure. A schematic is provided in Figure 3. This type of feature may also be staggered with a parapet wall to allow for a lower deck elevation wharf. An example of the parapet wall installation (albeit on a much larger scale) is available in Figure 4.

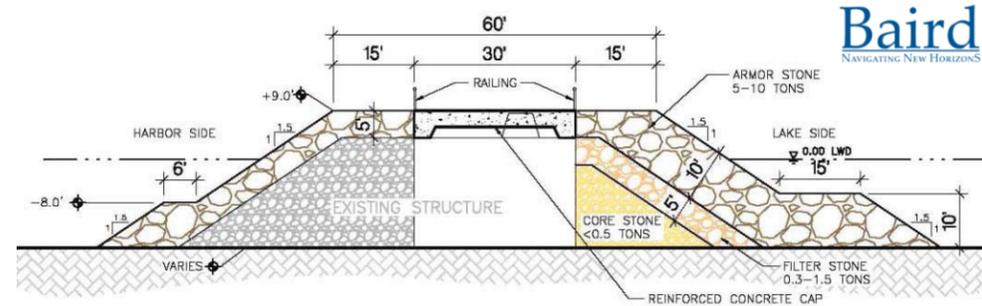


Figure 3. Crib Encapsulated with Rubblemound (Baird, 2008)



Figure 4. Concrete walkway with parapet wall on breakwater (Baird, 2008)

**2.3.3 Concrete Caisson**

Concrete caissons are essentially large hollow cubes of concrete that get ballasted with sand or quarry run fill. See Figure 5 for a schematic of a concrete caisson. For a concrete caisson to be installed, the previous crib would need to be removed and the lakebed prepared. Since the cribs

have been in place for many decades at Callander Bay, it is likely that lakebed preparations would be minimal for this site.

While a detailed cost estimate for a concrete caisson system has not been prepared, it is likely to be more expensive than the other alternatives presented to date.

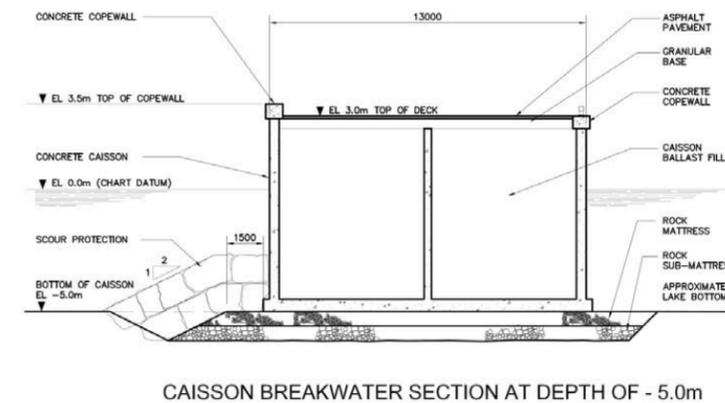


Figure 5. Concrete caisson schematic

**2.3.4 Comments regarding structure selection**

While the selection of the appropriate structure should be a marriage of cost and function considerations, it is likely that the cost estimates will govern the decision-making process. Cost estimates prepared to date have been approximate and while a sound first approximation, the certainty could be refined through the design process. The costs provided in the Mar-Land report include soft-cost approximations but do not include the full life-cycle cost of the installation. Section 2.4.1 of this report provides some guidance on life-cycle costs, including maintenance, of a typical coastal structure.

**2.4 Additional Considerations**

There are additional items to be considered in the installation of a new breakwater that have not previously been presented. These include life-cycle costs, marina requirements, and integration with waterfront planning.

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## 2.4.1 Life-Cycle and Maintenance Costs

There are two types of maintenance that should be considered with all coastal structures. These two types are generally broken into the categories of works that requires the mobilization of heavy machinery (structural rehabilitation) and works that do not require heavy machinery (Seasonal/Operations maintenance). Due to the significant costs, disruption, and ancillary damage to turf, parking lots, etc. it is generally accepted that structural rehabilitation will be done only very rarely; in many cases, this type of “maintenance” is undertaken as a full replacement of the existing structure.

### Seasonal/Operations Maintenance

This type of maintenance is related to regular operation repairs/maintenance such as painting, replacement of mooring hardware, grubbing splash aprons, minor repairs to lighting or other utility features. The costs are dependent upon the nature and complexity of the infrastructure that is to be installed on the breakwater. Sections 2.4.2 and 2.4.3 present some considerations regarding infrastructure on the breakwater. It is likely that for a basic level of infrastructure, the existing annual maintenance budget would be adequate.

### Structural Rehabilitation

Structural rehabilitation is related to the replacement or repair of significant elements of the coastal structure. With most coastal infrastructure projects, the cost of a major structural rehabilitation effort is comparable to the cost of full replacement. There are typically large mobilization and restoration efforts associated with this type of work, and engineering services are required. An example of this type of activity is the works being considered in the present report. Major rehabilitation efforts are generally considered rare and occur usually once or twice within a structure’s intended useful life.

One of the most popular methods for assessing the life-cycle cost of a project is to include the time-value of money in the assessment over the length of the planning horizon through a present-value assessment. An example of a possible present-value assessment has been conducted for this project using three different potential discount rates. Discount rates are heavily debated amongst economists, but values between 2 and 6% are commonly used at present. **This assessment is for example purposes only and is not to be used for planning decisions.** A full assessment is beyond Baird’s scope on this project. The assessment (a summary of which is provided in Table 1) assumes a \$2,000,000 capital installation cost, \$15,000 annual operations maintenance and a 50 year usable life for the wharf before significant structural rehabilitation is required.

Table 1. Present values for assumed life-cycle costs

Discount Rate	Present Values			Total
	Capital Expenditures	Operations Maintenance	Structural Rehabilitation	
2%	\$1,961,000	\$471,000	\$743,000	\$3,175,000
3%	\$1,942,000	\$386,000	\$456,000	\$2,784,000
6%	\$1,887,000	\$236,000	\$109,000	\$2,232,000

## 2.4.2 Marina requirements

The harbour basin created by the breakwater structure is effective at providing a marina and/or harbour of safe refuge for boaters. At present, there is a small private informally-operated marina within the basin. Without the breakwater, it is likely that the existing marina would experience difficulty with waves and ice.

Marina requirements vary with size and type of boat, as well as the type of user. There are two types of users considered for small marinas in Ontario: seasonal and transient. Seasonal boaters are those boaters that keep their boat at the marina throughout the boating season and use the marina as a base for accessing water-access cottages, or for heading out on day and overnight trips. Transient boaters are typically boaters from another location who stop by the marina from other marinas. Their use of the marina is short-term, usually less than one day although occasional overnight visits may be expected on large bodies of water.

Within these user-groups, there are three types of vessels: power, sail, and small craft. Small craft are typically, canoes, kayaks, or other craft that do not generally need significant docking space.

### Needs of seasonal users:

- Guidelines suggest allowing for 0.7 land-based parking spots for each seasonal boat (DFO, 1992);
- Allowance for an additional 0.2 to 0.4 land based parking spots per seasonal boat for visitors (DFO, 1992) is recommended
- Secure mooring area – vessels are usually left unattended for extended periods of time; and,
- Potable water system nearby for island cottagers, but not necessarily at each slip.

### Needs of transient users:

- Overnight transient boaters typically need shore power and prefer potable water at each slip;
- Day-use transient boaters are generally attracted by land-side amenities such as access to local restaurants, shopping, parks, and beaches;
- Day-use transient boaters are not normally equipped to carry garbage around. Access to garbage receptacles is a convenience that keeps the surrounding areas free of debris.

Additional notes regarding marina:

- The Chief Commanda II is a vessel operating on Lake Nippising and the French River. Although the vessel is stored in Callander Bay during the winter, the vessel does not normally stop at the Callander Bay wharf. Should a rehabilitated wharf be established, it may be possible to engage in discussions with the operators of the Chief Commanda II regarding stops at the Callander Bay Wharf. The Chief Commanda II requires depths of 2.44 m (~8ft) for operations.
- Depending upon the size and scale of future marina growth plans, a shorter breakwater may be feasible. In order to justify this, a formal marina growth plan as well as a diffraction study is recommended to optimize the length of the breakwater.
- Safety features such as ladders, life rings, and lighting are required.

#### 2.4.3 Integration with Waterfront Planning

In order to better integrate the wharf with the municipality, features and infrastructure on the wharf should be tied in with the municipal streetscape and trail network to draw boaters into the downtown area and to link the wharf with the social and cultural fabric of the community. Features that may be used to achieve this include:

- A consistent look and feel with garbage receptacles, light posts, pedestrian benches is recommended;
- Locally-appropriate materials should be used in cosmetic infrastructure selection. In Callander Bay, this includes timber decking rather than concrete or steel grating.
- Bring trails down to the waterfront;
- Link pedestrian access to waterfront parks, shopping, restaurants, and other community spaces.

### 3.0 ADDITIONAL WATERFRONT ISSUES

There are additional waterfront issues not related to the wharf or its tie-in to the local community. Coastal engineering elements of some common waterfront issues are provided in this section of the report. In this report Baird is providing some base knowledge relevant to the Callander Bay, however the scope of the current study does not include undertaking design work or preparing recommendations related to specific shoreline reaches or infrastructure.

#### 3.1 Parks and Shoreline Protection:

Shoreline protection is usually intended to protect land or infrastructure that is sensitive to erosion flooding, or other lake-based hazards. In some cases it may also be used to enhance a shoreline by creating conditions that may otherwise not be present – for example retaining a sandy beach in an area without significant sources of sand. In general erosion at parks and other municipally-owned property in Callander Bay should be monitored. Should erosion threaten existing land or infrastructure in an intolerable way, measures should be implemented to either relocate or protect the land and infrastructure.

At present, there is some basic steel sheet piling used as shoreline protection and turf delineation at Centennial Park (See Figure 6). During Baird's brief site visit, it did not appear as though this shoreline protection was in need of significant or immediate repair. It is recommended that erosion monitoring be undertaken in parkland areas without this protection.



Figure 6. Steel sheet pile shore protection in Centennial Park



# appendix 2: coastal engineering report

Baird & Associates

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Erosion monitoring may be relatively simple or complex depending upon the needs of the area. At Callander Bay, a simplified method is likely appropriate. This would likely involve establishing some base locations with permanent infrastructure/monuments from which field-tape measurements may be made to top of the shoreline's bank. It is important that the top of the shoreline's bank is used rather than the waterline as the waterline's positions changes dramatically with water level fluctuations.

## 3.2 Stormwater and Outfalls

Stormwater entering Callander Bay without treatment is a potential source for water quality challenges. It is recommended that Callander continually monitor their stormwater outfalls for signs of clogging, damages, and pollutants. Pollutants may indicate accidental or intentional releases of pollutants upstream in the storm water system that should be mitigated. In particular, storm water outfalls in Centennial Park and at Sunbeam beach should be monitored due to their proximity to swimming areas.

Excess silt and sediment load from storm water outfalls is also a potential problem as it could lead to sedimentation and navigation hazards in Callander Bay. Grit separation chambers and point-source protection may be effective at reducing this problem should it be present.

One effective management technique for storm water outfalls is to allow for final storm water polishing by planting localized strip-wetlands between the end of the outfall pipe and the receiving body. These frequently trap sediment and other pollutants before they can reach the receiving bodies and provide for increased biodiversity.

## 3.3 Private Riparian Shorelines

Private Riparian Shorelines present the greatest fraction of waterfront on Callander Bay. It is therefore important to support existing policy and consider strengthening policy surrounding the waterfront development at private shorelines in Callander Bay.

In Callander's Official Plan, Section 3.4 and its sub-paragraphs provide sound guidance surrounding development of waterfront areas, including private riparian reaches. While some of the elements are legally enforced by other levels of government (flooding and erosion hazards for example are the responsibility of North Bay – Mattawa Conservation Authority and fish habitat protection is the responsibility of Fisheries and Oceans Canada with the Conservation Authority), violations of these elements can negatively impact water quality and shoreline stability at adjacent properties, including municipal lands. Therefore, the municipality should continue to ensure that appropriate permits are in place from these partner organizations before providing building permits. Furthermore, individuals responsible for building permit enforcement should be aware and empowered to report violations to other agencies.

## 3.4 Intake Protection Zones

Ontario's source water protection program has delineated specific areas where additional rigor should be in place regarding waterfront development, contaminant release, and marine construction. Callander's wharf is within Intake protection zones one, two, and three (IPZ-1, IPZ-2, and IPZ-3) and therefore specific measures and co-ordination are required before works at the wharf can be undertaken (Hutchinson, 2010). It is recommended that the Conservation Authority and plant operator be listed as stakeholders in all wharf considerations.

## 3.5 Boat Launch

The existing boat launch at Callander's Wharf is in a state of disrepair and should be either rehabilitated or an additional location be made available elsewhere. Boat launches in Callander should be considered for year-round activities as they are ideal conduits for snowmobilers and ice fishing enthusiasts to regain access to land and enter town.

## 4.0 CONCLUSIONS

This report has been prepared to support the waterfront planning exercise being conducted by the Municipality of Callander and EDA Collaborative Inc. Its intent is to provide a brief summary of the coastal engineering related issues associated with the revitalization of the waterfront, including a review of existing reports regarding the Callander Wharf. No design work has been undertaken. The following paragraphs provide a brief summary of the report's findings.

The Mar-Land report on Callander's wharf was reviewed. Baird is in general agreement with the findings and recommendations in the Mar-Land report. Baird has provided additional remarks surrounding Callander Wharf issues not discussed within the Mar-Land report, in particular a method for estimating life cycle costs of Callander's wharf is presented along with conceptual values, however the final life cycle cost assessment was beyond the scope of this study. An additional point of significant importance is that the Callander wharf is located within the Intake Protection Zones for a drinking water intake. It is strongly recommended that the municipality include the water treatment plant operator and the North Bay – Mattawa Conservation Authority as stakeholders for all further decisions regarding the wharf.

Additional alternatives not considered in the Mar-Land report have also been provided. Design work to support these alternatives has not been undertaken, however conceptual schematics are provided. The additional alternatives include a hybrid steel sheet pile-rubblemound structure, a rubblemound encapsulation structure and a concrete caisson. The first two additional alternatives are considered viable however the concrete caisson is likely not viable from a cost perspective as well as not providing a look and feel that is consistent with Callander Bay's waterfront.

Marina guidance and considerations for the development of a marina are included in Section 2.4.2 of this report. It is possible that the wharf is longer than necessary to provide calm waters for a marina. A basic study could determine the necessary wharf length to provide this protection. If a shorter structure was acceptable, considerable cost savings could be realized.

Finally, a general discussion surrounding waterfront issues from a coastal engineering perspective is provided. Monitoring of erosion and outfall water quality is recommended.

## 5.0 REFERENCES

Baird (2008). *Chicago 2016 – Monroe Harbor Breakwater Modification Section 22 Planning Assistance to States*. Report prepared in conjunction with U.S. Army Corps of Engineers, Chicago 2016 Olympic Committee, and Chicago Park District.

DFO (1992). *Planning Guidelines for Recreational Harbours in Ontario*. A report of the Small Craft Harbours Branch prepared by Hough Stansbury Woodland Ltd., W.F. Baird & Associates Coastal Engineers Ltd. and Mar-Land Engineering Ltd. September 1992

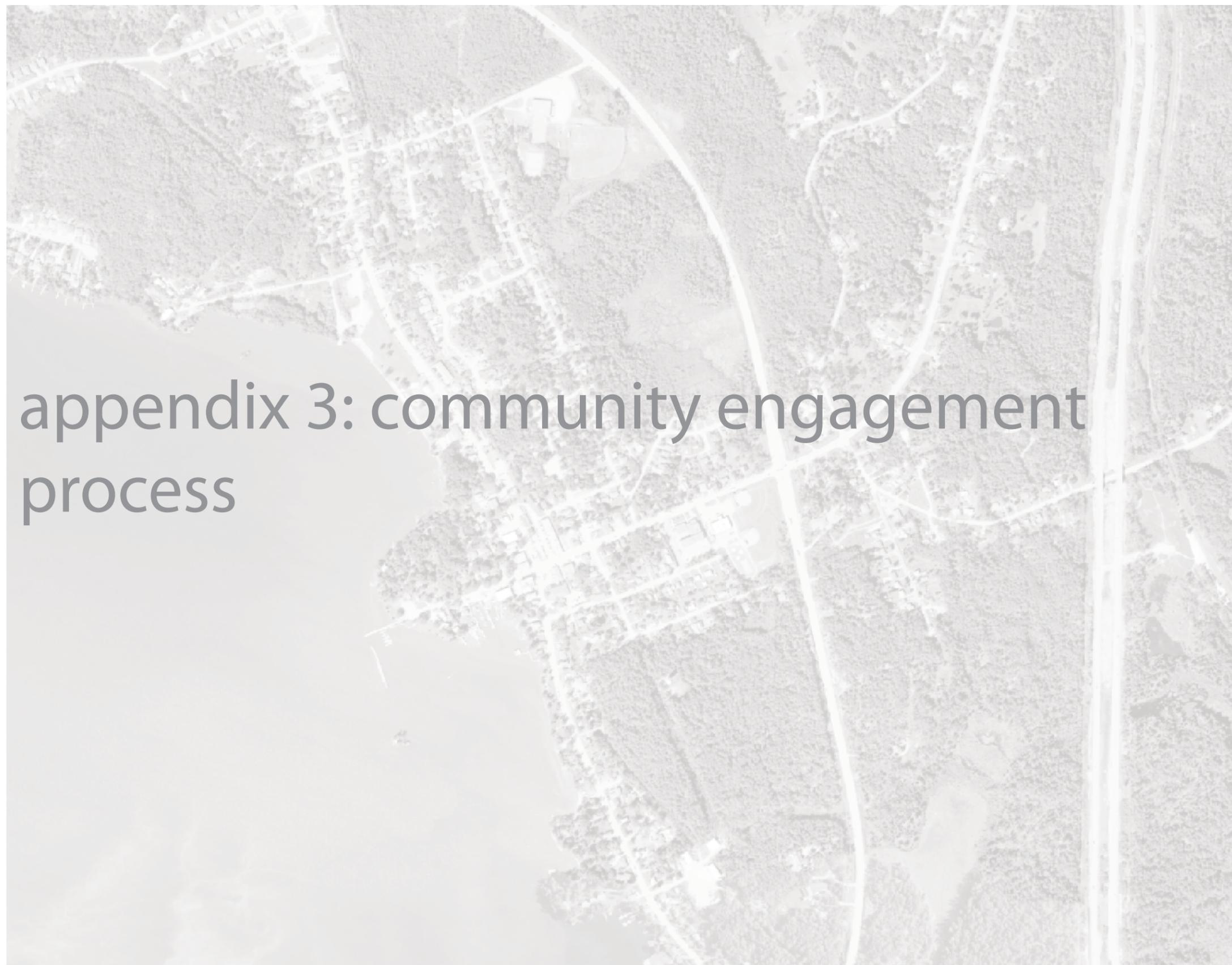
Hutchinson (2010). *Callander Drinking Water Source Protection Technical Studies Update*. Report prepared for North Bay – Mattawa Conservation Authority by Hutchinson Environmental Sciences Ltd. May 2010.

Mar-Land (2009). *Callander Wharf Structural Assessment & Costing*. Report prepared for the Municipality of Callander by Mar-Land Engineering Ltd. December 2009.



A3

appendix 3: community engagement  
process





### A3.1 Introduction and Overview

There are many stakeholders who have an interest in the future development of the Callander Downtown Waterfront Area. Government, industry and members of the community share a common interest in ensuring that the community of Callander further develops as a healthy and sustainable place to live, work and recreate.

Developing a successful Revitalization Master Design Strategy for the Callander Downtown Waterfront Area required that the community be engaged from the beginning. The process was as important as the product that has emerged.

From its inception, the project’s approach was rooted in promoting total inclusion and engaging members of the community in productive dialogue. The process was premised on a ‘whole team approach’ that fosters a sense of community ownership and civic pride. The community has shown a great deal of interest from the beginning of the process, with more than 30 community members coming out to attend the first community meeting on a Wednesday evening in March 2011. Since the outset, the process has continued to engage stakeholders in an evolutionary way.

Information secured from the community formed the basis for the analytical work completed by the Consulting Team. Throughout the process, information was shared, ideas were obtained and challenges were understood. Guiding principles and fundamental design elements emerged from the community through a number of focused community listening sessions, design charrettes and open houses.

### A3.2 Phases of the Community Engagement Process

At the outset, the process involved information and data gathering. The second phase involved community input into the design of the plan, and at each phase the process was iterative with members of the team reporting back to the community to ensure that their input was accurately interpreted and community ideas offered a basis for moving forward.

#### Information & Data Gathering Phase

**November 29th, 2010**

- Individual Interviews
- Community Leader Focus Groups

#### Community Design Phase

**March 2nd, 2011**

- Stakeholder Focus Groups
- Public Listening Session

**April 2011**

- Community Questionnaire

**May 18th, 2011**

- Community Design Charrette

#### Validation Phase

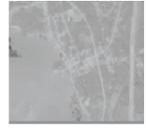
**July 6th, 2011**

- Community Review Open House

**Date TBD**

- Council Meeting for Approval

Community Engagement Process Diagram



# appendix 3: community engagement process

## A3.2.1 Information Gathering Phase

Given the importance of a community-driven approach, it was felt that a number of opportunities needed to be provided to allow members of the community to come forward with their ideas concerning the strengths, weaknesses and areas of opportunity for the Callander Downtown Waterfront Area. From the initial start up meeting on November 29th, 2010, members of the Consulting Team collaborated with Municipal staff to design an effective engagement process at the community level. The community engagement strategy that emerged was one that combined a number of critical components that included:

- One-on-One SWOT Interviews with Internal Staff, External Clients and Partners
- Stakeholder and Community Leader Focus Group Sessions
- Open Houses & Community Round Table Discussions
- Design Charrette with Diverse Representation from External Partners & Clients, Landowners, Township Staff and Members of the Public

At the very outset of the process, Municipal staff provided the Consulting Team with a list of key stakeholder informants – individuals that play a critical role in the community from a cultural, economic, social and environmental perspective. A standard list of interview topics was developed and administered in one-on-one interviews by members of the Consulting Team. These interviews were conducted in late November 2010.

Key informant interviews were conducted with twenty three (23) individuals. These interviews allowed specific individuals to share their views regarding the strengths, weaknesses, opportunities and threats facing the Callander Downtown Waterfront Area. Those interviewed included community leaders, recreation and service club representatives, business and restaurant owners, representatives from various sectors including education, arts, culture and heritage, marina and tourist operators, and representatives from the real estate and development community. These key informant interviews offered an initial glimpse of the issues and opportunities for Callander from an individual vantage point.



Callander Community Listening Session,  
March 2nd, 2011

### ***A3.2.2 Community Design Phase***

Following the key informant interviews, the Consulting Team conducted a number of focused dialogues. These larger group sessions were designed to capture the collective views of Municipal staff and community leaders as well as members of the business community and the broader public at large. Individual facilitated sessions were held with Municipal staff, Municipal Council, members of the Chamber of Commerce and a Public Listening Session was held on March 2nd, 2011. That initial Listening Session drew over 30 people – all with an interest in the future of Callander. A community questionnaire was mailed out in April to all property owners within the Municipality, garnering over 40 responses. The Municipality and the Consulting Team then hosted a Community Design Charrette on May 18th, 2011, to build upon the feedback received to date and gain insight into what the community's vision for the future of the Callander Downtown Waterfront Area. The Consulting Team also received ideas from the students at M. T. Davidson Public School, as part of a class assignment, regarding their vision for the future of Centennial Park and the Downtown Waterfront Area overall.

The individual interviews, focused facilitated sessions, community questionnaires and inputs from local school children, provided the Consulting Team with a number of insights into the issues and areas of opportunity for the Revitalization of the Callander Downtown Waterfront Area.

### ***A3.2.3 Validation Phase***

Throughout the master planning process, Municipal staff and members of the consulting team checked in with the community to ensure that suggestions were incorporated and ideas were understood. This process of continued validation allowed not only the community to remain involved but also continually kept up to date on the Consulting Team's progress.



*Community Design Charrette, May 18th, 2011*



# appendix 3: community engagement process

## A3.3 Key Findings

### A3.3.1 Community Strengths

The inputs collected from the community have provided the Consulting Team with crucial insider perspectives of the strengths and challenges within the Callander Downtown Waterfront Area. These discussions have enabled the Consulting Team to better understand the issues and opportunities for Callander’s core area.

The Callander Downtown Waterfront Area has many strengths:

#### **Location and Community Identity:**

Callander enjoys a strategic location, given its proximity to Highway 11 and North Bay. It is a quiet community with a small town feel, a bedroom community to neighbouring North Bay. It is a place where everyone knows their neighbours with a committed volunteer sector. It is a friendly and safe community with a growing artisan community.

#### **Accessibility and Aesthetics:**

Callander has excellent access to and from Callander Bay and beyond to Lake Nipissing, with a walkable downtown waterfront area. The community prides itself on the waterfront and unobstructed view and vistas of picturesque Callander Bay.

#### **Community Services:**

Callander is proud of its rich cultural and historical background, found today in the local museum and library. The core area offers a range of health services and some professional services providing residents with a high level of medical care. There are many vital retailers within the core area as well as a range of churches and schools. Callander is serviced by police and fire services vital for community safety. Seasonal rental properties are also abundant in this area providing a tourism base for the community.

#### **Recreational Amenities:**

Callander boasts a variety of both passive and active green space and parks with some space for special events and public access to the water with a number of public beaches for swimming. It has a recreational community centre, golfing facilities and yacht club, with trail connections for year round uses. Callander has excellent fishing conditions in all seasons and there are two boat launches for public use within the core area. The pier has the potential to become a recreational strength for Callander in the future.

#### **Land Use:**

Callander has experienced a steady improvement in pride of ownership in relation to private property. There is limited industrial activity and development allowing for an intermixing of residential uses, public facilities and commercial land uses. There is land available for potential development including vacant properties within the core area. Currently, there is a good variety of businesses within the core area.

#### **Community Strengths:**

- *Location and Community Identity*
- *Accessibility and Aesthetics*
- *Community Services*
- *Recreational Amenities*
- *Land use*

### A3.3.2 Challenges

While there were many strengths identified for the Callander Downtown Waterfront Area, a number of challenges also emerged. These include:

#### **Image and Identity:**

It was noted that there are some aesthetic issues within the Downtown Waterfront Area with a need for visual improvement in some areas. There was concern over the cleanliness of the core area and local beaches. There was also concern expressed about the condition of some properties within the core area. It was noted that there is a lack of spaces for social interaction and gathering within the core, as well as a need for information maps and signage, particularly for tourists and visitors.

#### **Safety:**

Safety for pedestrians within the Downtown Waterfront Area, particularly at the intersection of Main and Lansdowne Streets was identified as a concern. It was noted that sidewalks are in need of improvement especially for winter safety. A lack of adequate accessible parking within the core area was also identified as a specific point of concern.

#### **Infrastructure Servicing:**

The pier was a prominent point of concern, regarding the need for repair or rebuilding. A lack of sewage treatment capacity was identified as a major challenge to overcome as was limited supply of parking within the Downtown Waterfront Area. A lack of parking supply within the Downtown Waterfront Area was also identified as a concern.

#### **Community Services and Facilities:**

It was noted that there is a need for a larger library, possibly with outdoor space. The relocation of the fire hall was mentioned as a challenge as well. There was also concern expressed regarding the need for more activities for children within Callander.

#### **Environmental Features and Functions:**

The decreasing quality of the water (attributed to geese, sawdust, algae, etc.) within Callander Bay was identified as a challenge. The shallow water levels was also identified as a concern as this makes it difficult for boats to dock in some areas.

#### **Challenges:**

- *Image and Identity*
- *Safety*
- *Infrastructure Servicing*
- *Community Services and Facilities*
- *Environmental Features and Functions*

### A3.3.3 Initial Impressions, Opportunities and Issues

The key findings from the extensive community engagement process, combined with the initial impressions of the Consulting Team through site analysis, produced a number of recognizable opportunities, issues and challenges for the revitalization of the Callander Downtown Waterfront Area. These findings have been incorporated into the development of the final Master Design Strategy.